

Stratham Planning Board AGENDA July 13, 2022

Stratham Municipal Center

Hutton Room
Time: 7:00 PM

The public may also access this meeting at the date and time above using this conference call information. Please dial **1-800-764-1559** and input **4438** when prompted for a user pin/code. Please follow the Chair's instructions delivered at the meeting in order to register comments during the public meeting.

If at any time during the meeting you have difficulty hearing the proceedings, please e-mail mconnors@strathamnh.gov.

1. Call to Order/Roll Call

2. Approval of Minutes:

- **a.** June 16, 2022 Minutes
- **b.** April 20, 2022 Minutes

3. Public Hearing:

- **a.** Tulip Tree, LLC (Owner), Kyle & Sophie Saltonstall (Applicants) Request for approval of a site plan amendment to allow for modifications to the landscape plan associated with an event venue, non-profit lodge, and private school use at 61 Stratham Heights Road (Tax Map 5, Lot 81) approved by the Planning Board on March 6, 2019, Zoned Residential Agricultural.
- **b.** To amend Section 4.2.7 of the Site Plan Regulations for the purposes of adding application fees for applications submitted under the Route 33 Neighborhood Heritage District.

4. Public Meeting:

- a. Potential land donation of 18 Jana Lane to Town of Stratham
- **b.** Formation of the Pedestrian-Bicycle Sub-Committee of the Planning Board
- c. Discussion of Transportation Planning Priorities
- **d.** Master Plan Implementation
 - Walking, Biking, and Automobiles
- e. Miscellaneous Community Planning Issues

5. Adjournment

Full text of the agenda and related information can be found on file with the Stratham Planning Department and posted on the Town website at https://www.strathamnh.gov/planning-board. All interested persons may be heard. Persons needing special accommodations and /or those interested in viewing the application materials should contact the Stratham Planning Department at (603) 772-7391 ext. 180.



1 **Stratham Planning Board Meeting Minutes** 2 June 22, 2022 3 4 **Stratham Municipal Center** Time: 7:00 pm 5 6 7 8 **Member Present:** Thomas House, Chair Mike Houghton, Selectmen's Representative 9 10 Chris Zaremba, Regular Member Pamela Hollasch, Regular Member (by telephone) 11 John Kunowski, Alternate Member 12 13 14 Members Absent: David Canada, Vice Chair 15 **Staff Present:** Mark Connors, Town Planner 16 17 18 1. Call to Order/Roll Call 19 Mr. House called the meeting to order and took roll call. Mr. House appointed Mr. Kunowski as a 20 voting member for the meeting. 21 22 23 2. Approval of Minutes 24 **a.** June 1, 2022 25 The approval of minutes from June 1, 2022. Mr. House corrected the spelling of Mr. Zaremba's name in the draft minutes. Mr. Hougton made a motion to approve the draft minutes as amended. 26 Mr. Zaremba seconded the motion. All voted in favor. 27 28 **b.** March 16, 2022 29 Mr. Kunowski asked if the approval of minutes from March 16, 2022 is outstanding. The 30 minutes are not in the package and will be presented at the July 13, 2022 meeting. 31 3. Public Meeting: 32 33 a. Route 33 Heritage District and potential scheduling of public hearing for Route 33 Heritage 34 35 District application fees. 36 37 Mr. House asked Mr. Connors to provide an update. The Route 33 committee held their first meeting on June 16, 2022. The committee approved an application and rules and procedures as to 38

The timing will work well with the Planning Board meeting schedule. The committee will

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how the committee will be run. Their meetings will be held the first Wednesday of each month.

 prepare their comments within one week to be included in the packet for the following Planning Board's meeting. They discussed application fees. The Planning Board approves all Planning Board application fees, so the committee can only recommend the fee amounts. Planning Boards have to hold a public hearing to set the fees. Mr. Connors presented the recommended fees. Any application relating to a residential or agricultural property, that does not result in new housing units, would be a \$50 fee. This is also the base fee so anything that is non-applicable like demolition without new construction or an application for a commercial sign that does not meet the requirements would also fall under this amount. A residential application for new housing units would be \$100 plus \$75 per new housing units. Adding an ADU to a house would be \$175. A commercial application applying to a small area less than 1,500 square feet would be \$100. A commercial or mixed use application that is larger than 1,500 square-feet would be \$200. These fees would be in addition to the Planning Board fees. Mr. Zaremba asked if these fees are lower than fees in other parts of town to encourage development in the district, how do they compare to other parts of town? Mr. Connors replied that generally speaking, the fees are low, they are much lower than building permit fees. For example, a building permit application was just processed for the new medical office building and the building permit fee was \$13,000. The committee is trying to keep the fees nominal.

Mr. Zaremba asked if the intent is to encourage development by keeping the fees low. Mr. Connors replied yes. Mr. Kunowski stated that we still need to consider the administrative process of reviewing the applications, but agreed that the fees should be as low as possible. Mr. House stated that the goal is to cover the work of the planning staff. Mr. House stated that the second fee is standard. Mr. Houghton asked if any of this is pertinent to new construction, that this is all additions or modifications? Mr. Connors replied that it can be either. Mr. ? stated that one of the fees is for a new residential application. Mr. ? stated that the commercial application can include an addition. Mr. Connors said that if it a big development, like a 5,000 square foot development, the site plan fee is \$100 per 1,000 square feet. Mr. Houghton stated that if you put a value on a few hours worth of someone's time, it's not \$50, so he would advocate ensuring the fees cover costs. Mr. House asked if Mr. Connors is seeking approval tonight because two members are not present. Mr. Connors replied that the Board could schedule a public hearing and debate the fees at the hearing. The Board does not need to make a final decision on the fees tonight. We would advertise the intent to set fees at the hearing, but not publish the proposed fee schedule.

Mr. Kunowski asked if the same fee categories apply to development in other parts of town or are these categories applicable to the Route 33 Heritage District? Mr. Connors replied that applications in the Heritage District would need to pay these application fees which are only applicable to the Heritage District. But the fees have been kept low so as not to discourage applications. Mr. Kunowski asked if something was happening in another part of town, an application fee would not apply? Are there across the board application fees? Ms. Hollasch joined the meeting via telephone. Mr. Connors replied that there are across the board fees, for example a site plan for a new store would be \$250 for the base fee for the Planning Board plus \$100 per 1,000 square feet. That applies to any site plan of this nature regardless where it is located in town. Mr. Houghton is interested in seeing the actual costs for the Town. Mr. House asked Ms. Hollasch if she reviewed the proposed Heritage District application fees. Ms. Hollasch

replied only cursorily. Mr. House asked if Ms. Hollasch has any questions on the fees. Ms. Hollasch replied no.

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Mr. House asked Mr. Connors to go over the application form. Mr. Connors replied that it is similar to the site plan application. The owner must sign the application. If the applicant is a different person from the owner, they must also sign the application. We ask for permission to visit the property. Mr. Zaremba said the application looks straight forward. Mr. Zaremba asked once the application comes in, it is a two-step process that first goes to the Route 33 Heritage Committee and then come to the Planning Board? Mr. Connors replied yes. The Committee would give the Planning Board their meeting minutes with comments and recommendations, but the Planning Board ultimately makes the decision. Mr. House stated that there is a little bit of vetting before the project is submitted to the Planning Board. Mr. House stated that one of the goals is to help save time at the Planning Board. Ms. Hollasch has no questions on the application. Mr. House named the three members of the Heritage Committee: Alex Dardinski is the chair, Jeff Hyland is the co-chair, and Mr. House as the Planning Board representative. Mr. Connors said that Mr. Dardinski would like to send a letter to all property owners notifying them of the application process.

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Mr. Houghton made a motion to set the first public meeting date to set site plan application fees for the Heritage District as July 13, 2022. Mr. Kunowski seconded the motion. All voted in favor and the motion was approved.

b. Stratham Open Space Plan

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Mr. Connors discussed the presentation from the introductory meeting last week. He said he would briefly run through the same Powerpoint that was presented at that meeting.

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The Capital Improvement Program budgets about \$45,000 for Open Space and Connectivity Plan in addition to the Town's other funding towards land conservation. The Town has put between \$15,000 and \$35,000 toward land conservation from 2009 through 2019. The Town's commitment to Open Space conservation intensified in 2002 when the Town passed a \$5 million bond for open space preservation. A lot of land preserved in Stratham came out of that bond commitment. There is also some modest funding in the CIP for pedestrian improvements. Mr. Connors went through the recommendations from the 2019 Master Plan related to Open Space Planning. The Master Plan recommended Management Plans for individual large open space parcels. The Plan recommended a specific long range plan for Stratham Hill Park. The Town has started that effort thanks to the work of the Trail Management Advisory Committee (TMAC). The Plan recommends preserving the remaining historic buildings in Stratham and continuing to use Conservation Easements where appropriate. Mr. Zaremba asked if using Conservation Easements changes the tax base of the land? Mr. Connors replied yes because development of the property is limited. Typically, the Town purchases the development rights which reduces the value of the property. Mr. Connors showed how population in Stratham evolved. There was little impact from the Industrial Revolution. The population doubled in the 1980s. Approximately 1,700 acres of land are protected under Conservation or Agricultural Easements in Stratham. The NH Fish and Game owns 115 acres and the Town of Stratham owns 486 acres, excluding properties like the Town Hall. About 24% of land in Stratham is protected. There are also

protected open space subdivisions with smaller lots and open space set aside. However, not all of the open space parcels are formally protected via an easement or some other permanent tool. Mr. ? asked if the open space at Treat Farm is protected. Discussion amongst the members is that they believe there is a protected easement. Mr. Connors said yes, he stated that it was more in the 1970s and 1980s that land protection was not always formalized when these subdivisions were approved. Stratham has done a better job with protecting agricultural land compared to other towns in New Hampshire. Mr. Connors showed a map of the different protected lands. There are a number of utility easements as well on the open space land and at the meeting last week it was discussed if the Town could acquire some rights to that land for hiking and pedestrian connections. Steve Whitman attended the meeting last week to present different types of open space plans. The presentation was recorded and Mr. Connors is working on getting the video online. It would be good for Stratham to get an inventory of open space and make it available to the public that details what can be accessed and what cannot. Complete an analysis of our baseline and where we want to go and complete public outreach and planning for what comes next. Mr. House requested a paper copy in the library for people without computers. Mr. Connors reiterated that creating a map with symbols of what the public can and cannot due on lands is important.

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Mr. Connors presented the Open Space Plan for the City of Portsmouth. There is a Greenway from downtown to the protected land on the outskirts of town. Mr. Connors presented the Open Space Plan for Burlington, Vermont. There was a lot of outreach to people being better stewards of the land. Moose Mountain, New Hampshire got a grant for a Greenways plan with neighboring towns.

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There was a question as to what Board and Committee members want the plan to focus on? Responses included protecting agricultural land, prioritizing environmentally sensitive land, clarifying the Town's and landowners responsibilities and Conservation Commission's roles, and clarifying where residents can access public land. Ms. Hollasch suggested inventorying the open space based on how the land benefits us, for example, flood control, public recreation, education, wildlife habitat etc. Mr. House suggested including maintenance of the open space properties as part of the plan. Discussion continued on suggestions for mapping Stratham's conservation areas. Mr. Connors would like the Town to work with the property owners who have easements that don't require public access but previously allowed access to their land. Ideally the Town could work with the landowners to see what can Stratham to promote better stewardship and respect of the property so public access could be regained? Mr. House suggested also talking to landowners who are considering adding trails. Ms. Hollasch noted the complication with easements held by an entity who is different from the entity managing the land therefore there are two entities that might have an opinion on how the land is used and maintained. It was suggested a new temporary committee might be needed to look at some of the items discussed. Mr. Connors stated the RFP would be released soon with Planning Board input, then to the Select Board, and then public outreach in the Fall.

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c. Discussion of potential source water protection and stormwater (MS4) mitigation measures.

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Mr. Connors stated there is nothing new from the last meeting.

d. Master Plan Implementation

Mr. Connors stated this was generally covered in the PowerPoint presentation earlier in the meeting relative to the Open Space Plan but I have printed out the recommendations relating to conservation right from the Master Plan if the Board has any questions. There were no questions or additional discussion.

Miscellaneous Community Planning Issues

Mr. Connors said the Regional Planning Commission reached out to Stratham as they update the long-range transportation plan. There are two plans to keep in mind, a Ten Year Plan where all projects slated for the next ten years are budgeted for. The Portsmouth Avenue/Route 33 safety improvements in Stratham and Greenland the and Bunker Hill Avenue/Portsmouth Avenue signalization project is in the plan. Those projects are budgeted for and moving forward. The second is the long range transportation plan which is like a master plan. There are five projects in that plan -- the Portsmouth Ave pedestrian/bicycle improvements, reconfiguration of the Stratham Traffic Circle, Squamscott Road bicycle lanes, Frying Pan Lane/Portsmouth Avenue intersection improvements, and Marin Way intersection improvements. The RPC would like the Town's input as to what one of these projects represents a highest priority. This is something to keep on your radars, I will prepare more information for you for the next meeting.

e. Adjournment:

Mr. Houghton made a motion to adjourn at 8:30 pm. Mr. Zaremba seconded the motion. All voted in favor and the motion was approved.



TOWN OF STRATHAM

Incorporated 1716

10 Bunker Hill Avenue · Stratham, NH 03885 Town Clerk/Tax Collector 603-772-4741

Select Board/Administration/Assessing 603-772-7391 Code Enforcement/Building Inspections/Planning 603-772-7391 Fax (All Offices) 603-775-0517

TO: Planning Board Members

FROM: Mark Connors, Town Planner

FOR: July 13, 2022

RE: Request to Postpone – 61 Stratham Heights Road

The applicants for the 61 Stratham Heights Road Planning Board application have requested that the application be postponed to the August 17, 2022 meeting as they are expecting a child birth.

The Planning Board would need to take a vote to postpone the application to a date certain. Staff takes no objection to the request and has notified abutters who have contacted the Planning Department of the request to postpone.

The original staff report for the application, application materials, abutter comments, and communications from the Building Inspector/Code Enforcement for the Planning Board's reference.

On July 6, 2022 at 22:43, kyle@saltonstallfarm.com wrote: Mr. Connors,

As discussed in person yesterday, my wife is very, very pregnant. Our soon to be born son is already weighing in at 9 1/2 lbs according to a recent ultrasound. Sophie is scheduled for a C-section at 9am on the 22nd, but the doctors have informed us that this child could come at any moment. Sophie would prefer that this kid come right now.

Given the unknowable near future, we ask that our upcoming scheduled appearance before the Planning board be postponed until the August 17 meeting. We'd like to promise to make the meeting next week, but we are not in condition to do so at this time.

Very Respectfully,

K Kyle Saltonstall Co-Owner, Saltonstall Farm US Army Special Operations Veteran

cell: <u>(603)312-4806</u> 61 Stratham Heights Road

Stratham, NH 03885

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TO: Planning Board Members

FROM: Mark Connors, Town Planner

FOR: July 13, 2022

RE: Site Plan Amendment application, 61 Stratham Heights Road

Background Information:

On March 6, 2019, the Planning Board approved a site plan to allow the construction of site improvements to support an event venue at 61 Stratham Heights Road. On March 26, 2019, the Zoning Board of Adjustment approved a special exception to operate the event venue then classified as a 'non-profit lodge' and 'private school.' The use operates out of a barn on the property and the improvements included a driveway, parking lots, and landscaping, among other features.

The site plan that the Planning Board approved included a row of 35 arborvitae trees adjacent to one of the parking areas. Although at one point the landscaping was installed, it appears many of the trees died and no longer exist.

On February 25, 2022 and May 2, 2022, the Code Enforcement Officer mailed the property owners letters noting that the site was not operating in conformance with the approved site plan due to the absence of this landscaping. The Code Enforcement Officer's letter of May 2, 2022 provided the property owners a deadline of June 15, 2022 to install the landscaping or to submit an application to the Planning Board requesting an amendment to the site plan. The applicants have chosen the latter option and an application was timely submitted.

On July 7, 2022, the applicants submitted a request that the application be postponed to August, noting that they are expecting a baby this month. Since the application was already publicly noticed, the Board will need to vote to postpone the application to a date certain and that will serve as the public notice.

Application Information:

The applicants are requesting that the landscaping in question be replaced, as a short-term measure, with eight rows of sweet corn that they have already planted. The Saltonstalls have not provided a plan or photographs of the corn so it is difficult to visualize what they are proposing, however staff will attempt to visit the site to take photos in advance of the Planning Board meeting. The applicants have indicated that they are working with engineers and landscape architects to develop a more comprehensive site management and landscape plan "that will far exceed the intent of the

original site plan and Stratham's site plan regulations.," however they have not provided a timetable for when the plan will be completed or when it will be submitted to the Town for review.

Further, the applicants note the cost of installing landscaping and the fear that new landscaping will not survive. The applicants have also included a letter from a landscape architect, who requests "some time to consider the appropriateness of the proposed buffer in relationship to the other needs of the farm, the opportunity to entertain alternatives, and (at a minimum) the ability to delay any evergreen planting(s) till the fall to increase the chances of success."

In the application, Mr. Saltonstall notes that the original intent behind the landscaping was to "shield the Cooper residence [abutters to the use] from nuisance noise and light emitted from usage of our eco-parking area."

The Coopers have indicated that noise emitted from events and other disruptions associated with the use is a continuing nuisance. They have submitted a letter opposing the applicants' request which is included in the Planning Board packets.

Staff Recommendation:

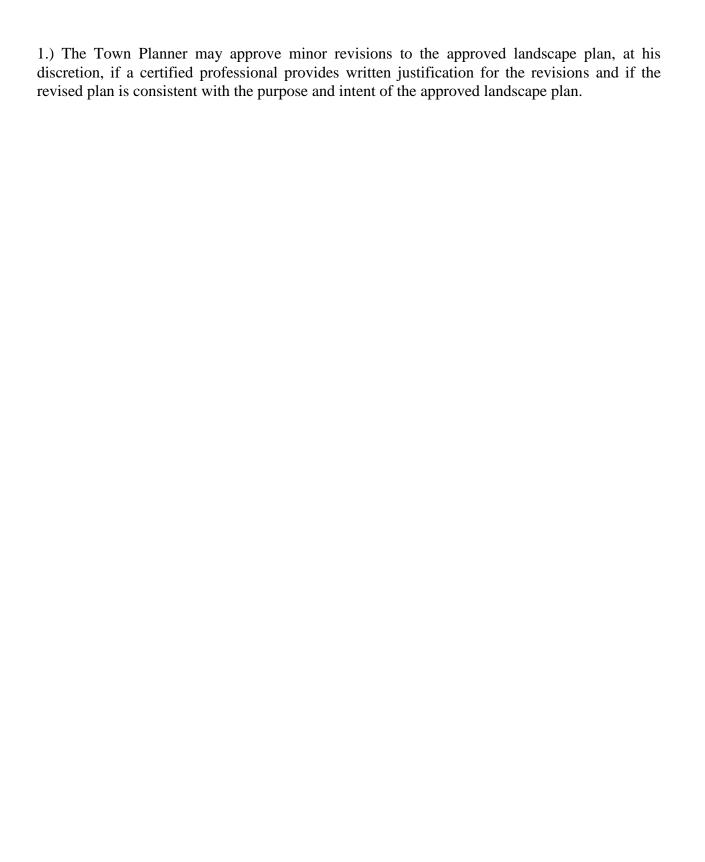
Staff does not believe corn is a reasonable alternative to the more comprehensive landscaping that was included in the approved site plan. It is important to note that the Town did not develop the landscaping plan; the applicants developed the plan likely with some input from abutting property owners. The Town is merely enforcing the approved site plan the applicants developed. The Town would welcome the opportunity to review a revised landscape plan, but the applicants have not provided one, only the promise to submit one at a later unspecified date. It could be years before one is submitted. It has been nearly five months since the Town first formally notified the applicants of this compliance issue. Staff believes that should have provided sufficient time for the applicants to develop and provide the Town a revised plan.

New Hampshire provides a good climate for a wide diversity of landscaping provided adequate site preparation is made, irrigation is installed, and that the plantings are maintained. The applicants have not provided any technical exhibits to indicate why the land is not appropriate for landscaping.

Because the applicants have not provided a viable revised plan, staff would recommend that the Board deny the request for an amendment to the plan and uphold the original site plan but provide the applicants a 60-day window in which to make the improvements. However, staff would recommend that the Board provide the applicants some additional flexibility and allow for some minor revisions to the landscaping plan at the discretion of the Town Planner if a certified professional develops an alternative plan that meets the intent of the original landscape plan (see language of Condition #1).

DRAFT MOTION:

I move that the Planning Board deny the request for a site plan amendment because the request is not in conformance with Section 5.2 of the Stratham Site Plan Regulations. Further, the Planning Board directs the Code Enforcement Officer to enforce the approved 2019 Site Plan for 61 Stratham Heights Road, but to withhold enforcement action for a period of 60 days to allow the applicants the opportunity to bring the property in compliance. This decision is subject to the following condition:





TOWN OF STRATHAM

INCORPORATED I 7 I 6 10 BUNKER HILL AVENUE• STRATHAM NH 03885 VOICE (603) 772-7391 • FAX (603) 775-0517 • www.strathamnh.gov

February 25, 2022

Tulip Tree LLC Attn: Sophie and Kyle Saltonstall 61 Stratham Heights Road Stratham, NH 03885

RE: 61 Stratham Heights Road, Map 5, Lot 81

Planning Board Notice of Decision dated 3/22/2019

Dear Mr. and Ms. Saltonstall,

The purpose of this letter is to follow up on the Planning Board's site plan approval decision referenced above (and attached). The board's decision was based on a site plan, recorded at the R.C.R.D., as Plan D-41746 (also attached), which illustrates thirty-five 6' to 10' tall evergreens planted on a berm along the westerly and northerly sides of a 37-space parking area. Although this work was completed, some of the plantings have since died. As the property owner, you are required to maintain the property in accordance with the approved site plan and the plantings that are no longer standing or in poor health must be replaced. Please make preparations to perform this work in the Spring.

Please respond to this letter as soon as possible so that you can present me with a plan to bring the property back in compliance with the approved site plan.

Please do not hesitate to contact me if you have any questions.

Sincerely,

Jim Marchese, Code Enforcement Officer

Cc: Mark Connors, Town Planner



TOWN OF STRATHAM

INCORPORATED I 7 I 6
10 BUNKER HILL AVENUE• STRATHAM NH 03885
VOICE (603) 772-7391 • FAX (603) 775-0517 • www.strathamnh.gov

May 2, 2022

Tulip Tree LLC Attn: Sophie and Kyle Saltonstall 61 Stratham Heights Road Stratham, NH 03885

RE: 61 Stratham Heights Road, Map 5, Lot 81

Planning Board Notice of Decision dated 3/22/2019

Dear Mr. and Ms. Saltonstall,

The purpose of this letter is to respond to an April 28, 2022 email from Kyle Saltonstall who was responding to a site visit by the office earlier in the day. The office appreciates Kyle's quick response to the questions raised during the site visit.

As stated in the 2/25/2022 letter you are required to maintain the property in accordance with the approved site plan by the Planning Board dated 3/22/2019. Spring of 2022 is now upon us. This office believes that a fair and reasonable expectation is to the have the property in compliance by 6/15/2022. If your intention is not to regain compliance or to modify the approved plan you will need to file for an amendment to the site plan to the Planning Board by 6/15/2022. This will allow for the Planning Board to determine if your intended site modifications meet the goals established by the prior approval.

Please do not hesitate to contact me if you have any questions.

Sincerely,

Jim Marchese, Code Enforcement Officer

Cc: Mark Connors, Town Planner

Location: 61 STRATHAM HEIGHTS COAD Project Description: Please see affactual lefter + Supporting documents Zone: RA New Industrial / Commercial Square Footage: or Number of Residential Units: Applicant: Name: SOPHIE SALTONSTALL Phone: 603 475 2144 By		Town of Stratham, NH Site Plan Review Application Map# 5 Lot# 8
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Name: SOPHIE SALTONSTALL Phone: 603 475 2144 Address: 61 STRATHAM HEIGHTS ROAD Owner: Name: SAME AS ABOVE Phone: Fax: By Phone: Company: Fax: Address: ABOVE Phone: Fax: Address: Agent: Fax: Address: By Saltonstall Farm Lic Fax: Address: By Saltonstall Farm Common Stratham, and are agreeing to allow agents of the Town of Stratham to conduct inspections, during normal business hours, of your property, to ensure compliance with all Stratham Toming and Site Review regulations while your application is under consideration and during any construction and operational phases after approval is granted. The Signor shall be the owner or the signor shall provide a letter signed by all the property owners giving the signor permission to represent the owner in presentation of this application. Signed: Date: 5150.00 plus Abutters Fee: Abutters X\$8.00 = \$40.00 Site Review Fee*: \$		or Number of Residential Units:
Company: TULIP TREE LLC Fax: By Address: 61 STRATHAM HEIGHTS ROAD Owner: Name: SAME AS ABOVE Phone: Company: Fax: Address: Agent: Contact Name: LYLE SALTONSTALL Phone: 603 312 4806 Company: SALTONSTALL FARM LLC Fax: Address: 61 STRATHAM HEIGHTS ROAD Email Address: HYLE SALTONSTALL FARM OOM By signing this application, you are agreeing to all rules and regulations of the Town of Stratham, and are agreeing to allow agents of the Town of Stratham to conduct inspections, during normal business hours, of your property, to ensure compliance with all Stratham Zoning and Site Review regulations while your application is under consideration and during any construction and operational phases after approval is granted. The Signor shall be the owner or the signor shall provide a letter signed by all the property owners giving the signor permission to represent the owner in presentation of this application. Signed: Date: 6-15-2022 Fees: Notification Fee: \$150.00 plus Abutters Fee: 5 Abutters X \$8.00 = \$40.00 Site Review Fee*: \$		Applicant: Name: SOPHIE SALTONSTALL Phone: 603 475 2144 JUN 15
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Owner: Name: SAME AS ABOVE Phone:		
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construction – with a minimum of one-hundred (\$100) dollars.		Notification Fee: $$150.00$ plus Abutters Fee: 5 Abutters $X $8.00 = 40 .00
· · ·		Site Review Fee*: \$00 One-Hundred (\$100) dollars for each 1,000 square feet of building
		construction – with a minimum of one-hundred (\$100) dollars.

See Section 4.2.7 of the Site Review Regulations for fee schedule.
*Additional fees may be charged to cover inspection and review cost.

For Office Use Only		
Date Application Received:	Total Fees Collected with Appli	cation: \$00
Abutters List Received:	Check List Received:	_
PB Hearing Date:	Notice Date:	PB Jurisdiction Acceptance Date:

K. Saltenstall 2BA Abuters

BARR, DAVID P BARR, ANNEY. 7 ORCHARD HILL ROAD STRATHAM, NH 03885

BENEFIELD KUNOWSKI FAMILY BENEFIELD, RICHARD D. -TR KUNOWSKI, JOHN F. -TRUSTEE 55 STRATHAM HEIGHTS STRATHAM, NH 03885

COOPER FAMILY REVOCABLE T COOPER, JOSHUA M.& KIMBER 9 ORCHARD HILL ROAD STRATHAM, NH 03885

MCCARTHY, NINA MCCARTHY, STEPHEN 54 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885

MCDONNELL, MICHAEL C. & C TRUSTEES OF MCDONNELL FAM P.O. BOX 186 SEABROOK, NH 03874

5 TULIP TREE LLC 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885

K Kyle Saltonstall 61 Stratham Heights Road, Stratham, NH 03885 (603)312-4806 Kyle@SaltonstallFarm.com



Dear Stratham Planning Department and Planning Board,



I am writing to ask for an amendment to our current site plan for our property at 61 Stratham Heights Road. The site plan originally called for evergreen trees (highlighted in red) to be planted on the north side of our eco-parking area (highlighted in green).



Mr. Marchese informed me via in-person meeting and official letter that we must either "maintain the property in accordance with the approved site plan" or "file for an amendment to the Planning Board." Given no other alternatives, we have chosen to pursue the latter option for the following reasons:

- ...the insufficiency of the previously approved landscaping to achieve the intents of the original site plan and Stratham's Site Plan Review Regulations,
- ...our ongoing consultation with a landscape architect and site plan engineer to develop a whole property design which will implement landscaping and property management stratagem that will far exceed the intent of the original site plan and Stratham's Site Plan Review Regulations,
- ...and the significant costs associated with reimplementing the previously approved landscaping in question, knowing that the trees would likely not be permanent.

The insufficiency of the previously approved landscaping to achieve the intent of the original site plan and Stratham's Site Plan Review Regulations

The original intent behind placing evergreen trees along the north side of the ecoparking area was to shield the Cooper residence (highlighted in orange) from nuisance noise and light emitted from usage of our eco-parking area. Stratham's Site Plan Review Regulations Section 5.2-C-2 states that "Landscaping... must be provided with proper regard to adjacent properties... including interior landscaping of parking areas." Because of this, the more than 40' of dense vegetation (highlighted in pink and blue) between properties was deemed inadequate and we added landscaping around our eco-parking area.



Unfortunately, after the trees required by the current site plan were planted in June of 2020, they did not fare well. This is despite using the best practices outlined in Stratham Site Plan Review Regulations Section 5.2-K, namely deep tillage and incorporation of organic matter. I suspect this is due to them being...

- ...located in an area generally inhospitable to evergreen trees. The ecoparking area is filled with several feet of exceptionally well drained gravel; the eco-parking area and its border are thus bone dry unless frequently irrigated. Frequent irrigation is not plausible during the winter months when our irrigation is shut off due to frozen ground conditions, yet evergreen trees still require soil moisture in those months.
- ...planted at a terrible time of year. June is possibly the worst time of year to plant evergreen trees, an activity best suited for the early spring as soon as soil is workable. Even fall or winter are more suitable times to plant evergreen trees, but the heat of summer will put incredible stresses on evergreen trees that they are not prepared for during transplant.
- ...killed by an unknown factor that I am unaware of. Plant pathology is something I'm still new to.

If my assumptions are correct and this area is poorly suited for evergreen trees, replanting this area with trees doomed to wither and die will not only fail the intent to shield our abutters from light or noise generated from the eco-parking area, but it will also fail Stratham's Site Plan Review Regulations Sections 5.2-C-5-b and 5.2-C-5-c, which state that "Plant varieties shall be selected to ensure long-term survival" and "Plant varieties shall be selected so as to reduce long-term maintenance." Furthermore, the trees as drawn on the site plan are drawn linearly along the edge of the eco-parking area, but Stratham's Site Plan Review Regulations Section 5.2-C-10 states that landscaping should "...be laid out in informal drifts rather than formal rows," and that "Linear solutions shall be avoided wherever possible." Finally, the approved plan did not, as is required by Stratham's Site Plan Review Regulations Section 5.2-C-16, "Use a variety of species to... Create visual depth in plant massing by layering plants of various textures, sizes and colors," or "Include flowering or fruiting species for color, interest, and wildlife habitat where appropriate."

Our ongoing consultation with a landscape architect and site plan engineer to develop a whole property management plan which will implement landscaping and property management stratagem that will far exceed the intent of the original site plan and Stratham's Site Plan Review Regulations

The previous plan was developed by a local engineering firm. While they are most certainly one of the best firms around for site planning and while we are 100% satisfied with their designs for the eco-parking area and related site drainage, we don't believe that an engineering firm is always the best choice for landscape design. Moving forward, we've chosen to enlist the services of a landscape architect to help us landscape much of our property on the north side of Stratham Heights Road.

I have attached a letter from Cornelius Murphy, the Principal Landscape Architect from Whole Systems Design Collective, LLC. Our conversations with Mr. Murphy have centered our values in property management, which include soil health regeneration, sustainable water use & reduction of run-off, creation of wildlife habitat, and beautification of the property. We have also stressed the need for landscaping to create a visual screen and noise buffer between our property and our abutters.

Although the process of generating a high quality property management plan can be frustratingly slow, especially during COVID times, we feel confident that the wait will be worth it. If we take the time to create a well-designed plan now, we believe that we can create a very special space that benefits our town far into the future. We also believe that the only way to do this correctly is by enlisting the aid of seasoned experts who are able to correctly interpret pertinent regulations while also designing a durable system that meets our needs.

The significant costs associated with reimplementing the previously approved landscaping in question, knowing that the trees would not be permanent

Twenty-five 6' evergreen trees at wholesale costs will likely amount to over \$3,000 in trees alone. Add to this the cost of irrigation lines, soil amendments, and the labor to plant the trees, and I feel confident that my out-of-pocket costs would be greater than \$5,000. If I believed that these trees would survive a summer planting and would meet the intent of the original site plan, \$5,000 would clearly be a reasonable cost. However, I believe that this would be a \$5,000 expenditure that would fail to screen our eco-parking area from our abutters, fail to meet the requirements set forth in Stratham's Site Plan Review Regulations, and would genuinely be an unnecessary economic stress on one of Stratham's few remaining agricultural operations.

Our Proposal for a Site Plan Amendment

Seeing as we are currently working on a superior plan with long term durability — yet are still very interested in meeting the intent of the original site plan in the meantime — we are asking for the approval of the Planning Board to amend the site plan to allow us to continue maintaining the area in question using a more "farm-ish" practice. The area immediately north of our eco-parking area is currently growing eight closely spaced rows of sweet corn. The plants are well on track to meet the adage of "knee-high by the Fourth of July," and are well fertilized and well irrigated. Anyone who has visited the Scamman Farm's corn maze knows how effective a corn wall can be at screening sound and light, and we have planted the area far thicker that the walls in the Scamman Farm corn maze. We expect that our corn will be well over head height by the time that our season gets busy in September and October.

We are currently expecting our second child to be born any day now and have worked to keep our schedule clear of any responsibilities for the summer. Knowing this, the only use of our barn, and thus the eco-parking area, will be for a community Yoga class on Mondays. The instructor uses a novel system to instruct her students; they all wear headphones tied into her microphone. It is quite literally silent, save for the gently exhalations of the yogis. The Yoga class is over by 7 pm, well before sunset. Although the corn barrier will still be growing to its maximum height during these months and will not provide a complete screen, we believe that it will provide a better screen and be less disruptive to our abutters than tilling the soil, amending the soil, and planting twenty-five trees.

This is clearly an ongoing conversation between my family, our abutters, and the Town of Stratham. To best assist Stratham's Planning Department and Planning Board in helping us make decisions which will best improve our neighborhood and our town, we have included maps and photographs which show the physical locations and conditions of the areas in question. While we understand that sometimes no simple solutions can be found, we hope that given enough time, all parties can come to a compromise that will allow us to exist and thrive as healthy and active participants in our community.

Very Respectfully,

K Kyle Saltonstall

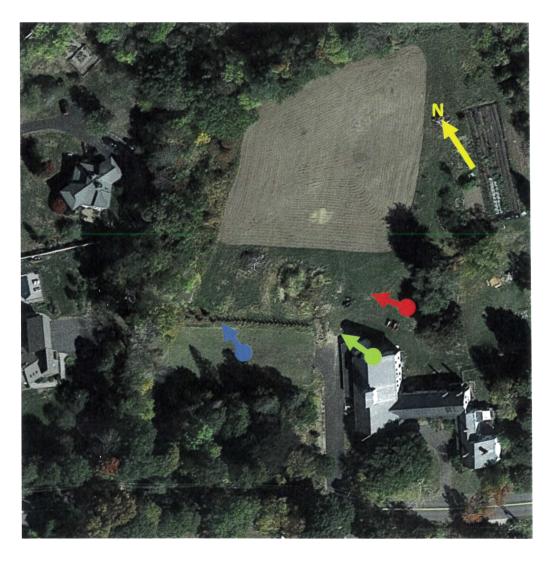


This map is included for the purpose of understanding changes to the property since the satellite images were taken in October of 2021.

- The area highlighted in red shows twenty-five 6' arborvitae trees. These trees died and are no longer in this area.
- The areas highlighted in blue shows a large mound of soil displaced during the construction of our parking area and a brush pile. Both the dirt and the brush have been removed and this area is currently covered in a lush field of clover and fescue.
- The area highlighted in green shows a bare dirt field. This area is currently planted in a cover crop consisting of phacelia, marigold, and cosmos, plant species selected for their ability to improve soil conditions and offer beauty as they bloom throughout the summer and fall.



This map is included to show the cone of noise emitted from both barn doors when fully open. Highlighted in orange is the Cooper Residence.



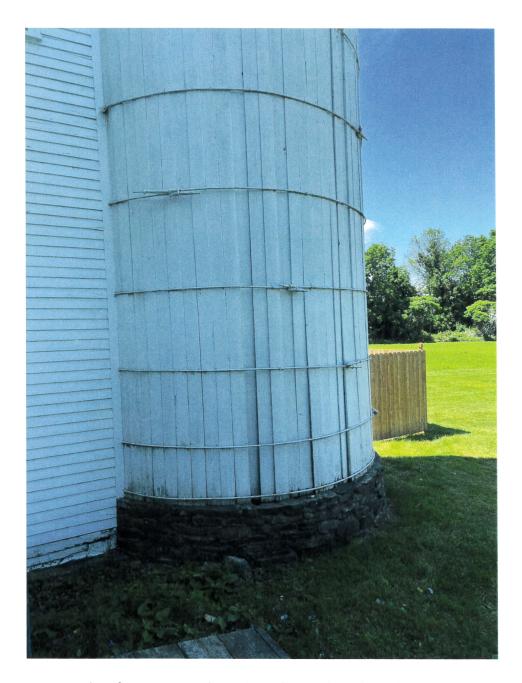
This map show the locations and directions from which the photos on the following three pages were taken.



Blue arrow, taken on June 15, 2022 from the center of the eco-parking area, facing directly at the Cooper Residence.



Red arrow, taken on June 15, 2022 from our lawn, facing directly at the Cooper Residence.



Green arrow, taken from our rear barn door, facing directly at the Cooper Residence.





Stratham Planning Board Stratham Municipal Center

10 Bunker Hill Avenue Stratham, NH 03885

June 14, 2022

Dear Stratham Planning Board Members,

We are writing to express our support for Saltonstall Farm regarding their efforts to mitigate noise and viewshed influences on neighboring properties and the concerns over a previously proposed planting buffer. Kyle & Sophie have recently engaged our firm to provide them with a comprehensive landscape plan for the area surrounding the north side of the event barn, an area that includes the planting buffer in question. We are in the early phases of design but hope to have a plan in place by the fall that would address any and all of the outstanding concerns but in a way that better meets the needs of the farm compared to the previously proposed planting.

We understand the alternative to waiting for our work to be complete would be to plant a stand of evergreen trees this summer. While we can appreciate the intent of a quick resolution, the plantings are much more likely to fail given the time of year which would leave the issues still unresolved. We are advocating for some time to consider the appropriateness of the proposed buffer in relationship to the other needs of the farm, the opportunity to entertain alternatives, and (at a minimum) the ability to delay any evergreen planting(s) till the fall to increase the chances of success.

Thank you for your time and consideration. Please let us know if there is anything else we can do to assist in this matter

Cheers,

Cornelius Murphy // RLA
Principal and Co-Founder
Whole Systems Design Collective, LLC

CLIENT

SOPHIE ROBINSON ROBINWOOD CENTER 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885

CIVIL ENGINEER

EMANUEL ENGINEERING, INC. 118 PORTSMOUTH AVENUE, SUITE A202 STRATHAM, NH 03885

LAND SURVEYOR
RICHARD PARKER
146 DRINKWATER ROAD
KENSINGTON, NH 03833

ARCHITECT

AMY DUTTON HOME 9 WALKER STREET KITTERY, ME 03904

SITE PLAN FOR THE ROBINWOOD CENTER

STRATHAM TAX MAP 2 LOT 14 & TAX MAP 5 LOT 81
EXETER TAX MAP 67 LOT 3 & TAX MAP 67 LOT 3-1
61 & 62 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885



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3	JULY 9, 2019	FORA	PPROVAL		-	
2	FEB 28, 2019	FOR A	PPROVAL			
1	JAN 16, 2019	FOR A	PPROVAL			
ISS.	DATE:	DESCRI	PTION OF ISS	UE:		снк
DRA	WN: JJM		DESIGN:	JJM		
CHECKED: BDS			CHECKED:	BDS		

EMANUEL

© structural consultants, land planners
118 Portsmouth Avenue, A202
STRATHAM, NH 03885
P: 603-772-4400 F: 603-772-4487
WWW.EMANUELENGINEERING.COM

SOPHIE ROBINSON
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

TITLE:

1" = 1,000 '

COVER SHEET FOR ROBINWOOD CENTER 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885

PROJECT: SCALE: SHEET: 18-150 AS SHOWN COVER

PROJECT DRAWING SET:

COVER SHEET

1461-2 PLAT OF LAND FOR DR. HENRY SALTONSTALL

C1 EXISTING CONDITIONS

C2A SITE PLAN

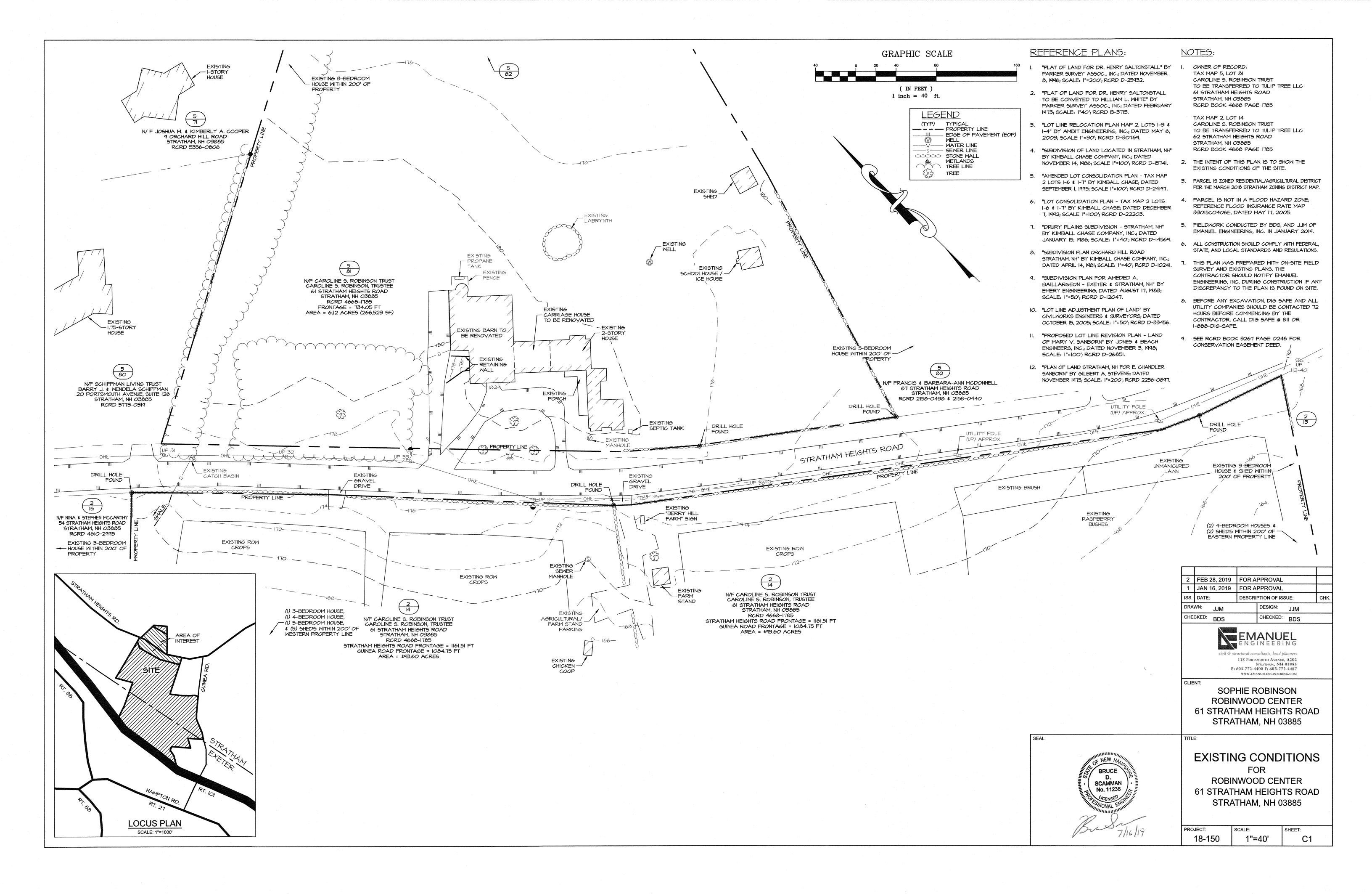
C2B SITE PLAN

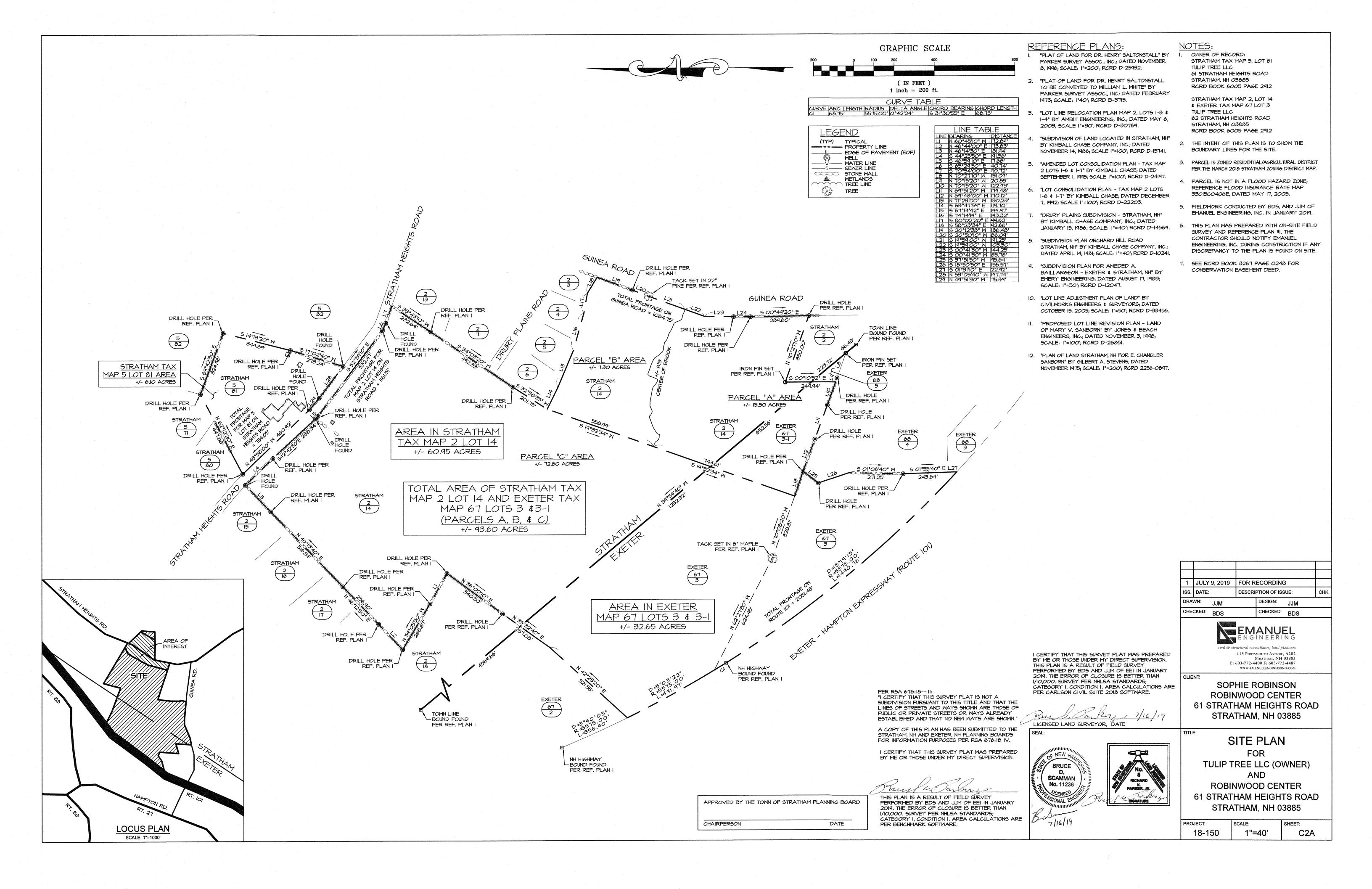
GRADING & DRAINAGE PLAN

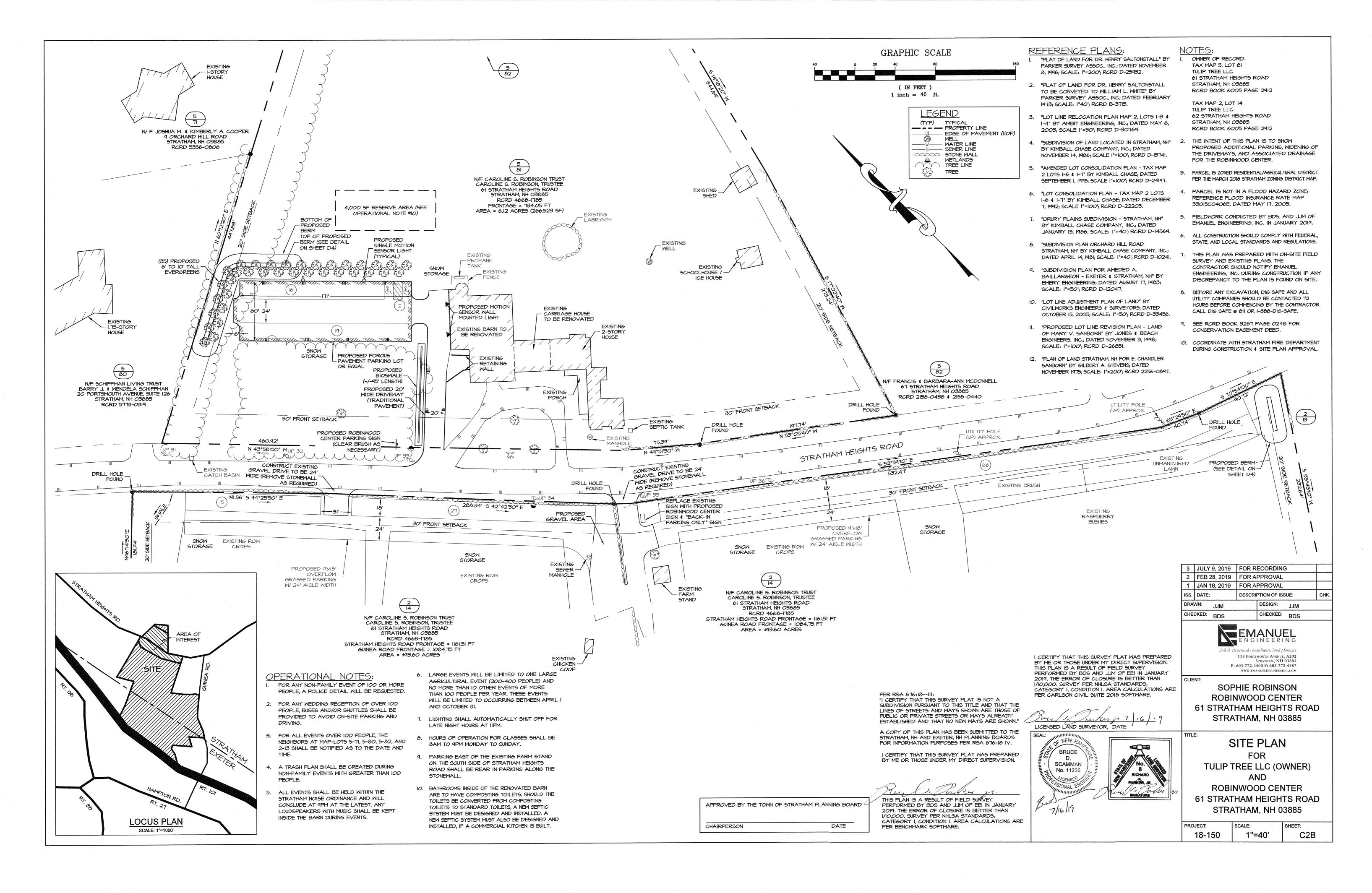
D1 & D2 NOTES
D3 & D4 DETAILS

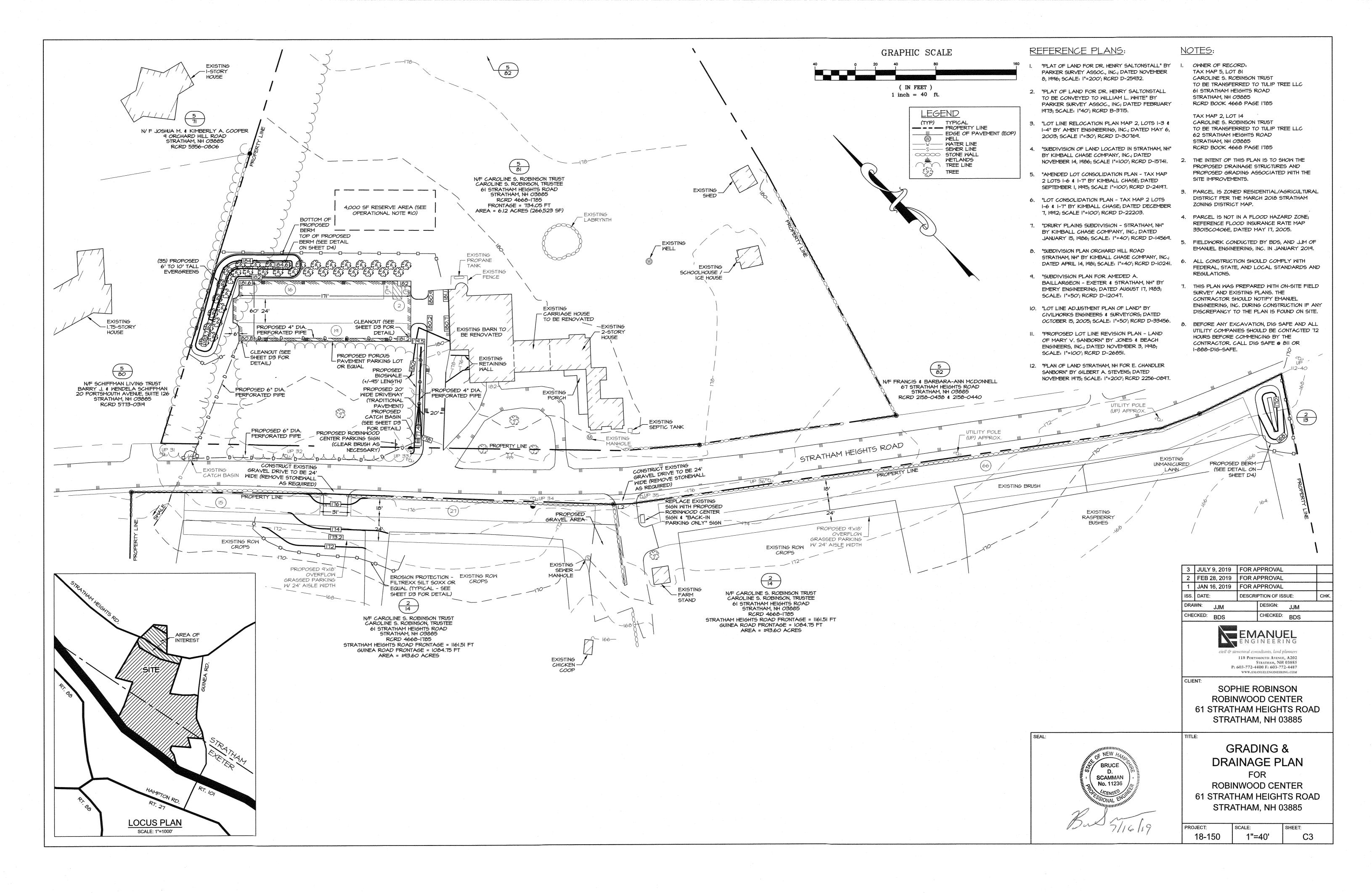
A12 ARCHITECTURAL ELEVATION

0008232









EROSION AND SEDIMENTATION CONTROL CONSTRUCTION PHASING AND SEQUENCING:

- SEE "EROSION AND SEDIMENTATION CONTROL GENERAL NOTES" WHICH ARE TO BE AN INTEGRAL PART OF THIS PROCESS.
- 2. INSTALL SILT FENCING AND/OR HAY BALE BARRIERS AS PER DETAILS AND AT SEDIMENT MIGRATION.
- 3. CONSTRUCT TREATMENT SWALES, LEVEL SPREADERS AND DETENTION STRUCTURES AS DEPICTED ON DRAWINGS
- 4. INSTALL TEMPORARY GRAVEL CONSTRUCTION ENTRANCE(S) AS PER DETAIL AND AT LOCATIONS SHOWN ON THE DRAWINGS. MAINTAIN (TOP DRESS) REGULARLY TO PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC STREETS.
- 5. STRIP AND STOCKPILE TOPSOIL. STABILIZE PILES OF SOIL CONSTRUCTION MATERIAL.
- 6. ROUGH GRADE SITE. INSTALL CULVERTS AND ROAD DITCHES.
- 7. FINISH GRADE AND COMPACT SITE.
- 8. RE-SPREAD AND ADD TOPSOIL TO ALL ROADSIDE SLOPES. TOTAL TOPSOIL THICKNESS TO BE A MINIMUM OF FOUR TO SIX INCHES.
- 9. STABILIZE ALL AREAS OF BARE SOIL WITH MULCH AND SEEDING.
- IO. RE-SEED PER EROSION AND SEDIMENTATION CONTROL GENERAL NOTES.
- II. SILT FENCING AND HAY BALES TO REMAIN AND BE MAINTAINED FOR TWENTY FOUR MONTHS AFTER CONSTRUCTION TO INSURE ESTABLISHMENT OF ADEQUATE SOIL STABILIZATION AND VEGETATIVE COVER. ALL SILT FENCING, HAY BALES AND TRAPPED SILT ARE THEN TO BE REMOVED FROM THE SITE AND PROPERLY DISPOSED OF.
- 12. PERIMETER CONTROLS SHALL BE INSTALLED PRIOR TO EARTH MOVING OPERATIONS.
- 13. PONDS AND SWALES SHALL BE INSTALLED EARLY ON IN THE CONSTRUCTION SEQUENCE -BEFORE ROUGH GRADING THE SITE.
- 14. ALL DITCHES AND SWALES SHALL BE STABILIZED PRIOR TO DIRECTING RUNOFF TO THEM.
- 15. ALL ROADWAYS AND PARKING LOTS SHALL BE STABILIZED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.
- 16. ALL CUT AND FILL SLOPES SHALL BE SEEDED/LOAMED WITHIN 72 HOURS OF ACHIEVING FINISH GRADE.
- 17. ALL EROSION CONTROLS SHALL BE INSPECTED WEEKLY AND AFTER EVERY HALF-INCH OF RAINFALL

WINTER CONSTRUCTION NOTES (OCTOBER 15 TO MAY I):

- ALL PROPOSED VEGETATED AREAS WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED BY SEEDING AND INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 3:1, AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE, SECURED WITH ANCHORED NETTING, ELSEWHERE. THE INSTALLATION OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR ON FROZEN GROUND AND SHALL BE COMPETED IN ADVANCE OF THAW OR SPRING MELT EVENT.
- 2. ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER 1. OCTOBER 15TH, SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS.
- 3. AFTER OCTOBER 15TH, INCOMPLETE ROAD OR PARKING SURFACES, WHERE WORK HAS STOPPED FOR THE WINTER SEASON, SHALL BE PROTECTED WITH A MINIMUM OF 3 INCHES OF CRUSHED GRAVEL PER NHDOT ITEM 304.3.

GRASS SWALE MAINTENANCE:

- I. TIMELY MAINTENANCE IS IMPORTANT TO KEEP THE VEGETATION IN THE SWALE IN GOOD CONDITION. MOWING SHOULD BE DONE FREQUENTLY ENOUGH TO 5. KEEP THE VEGETATION IN VIGOROUS CONDITION AND TO CONTROL ENCROACHMENT OF WEEDS AND WOODY VEGETATION, HOWEVER, IT SHOULD NOT BE MOWED TOO CLOSELY SO AS TO REDUCE THE FILTERING EFFECT. FERTILIZE ON AN "AS NEEDED" BASIS TO KEEP THE GRASS HEALTHY. OVER FERTILIZATION CAN RESULT IN THE SWALE BECOMING A SOURCE OF POLLUTION.
- 2. THE SWALE SHOULD BE INSPECTED PERIODICALLY AND AFTER EVERY MAJOR STORM TO DETERMINE THE CONDITION OF THE SWALE. RILLS AND DAMAGED AREAS SHOULD BE PROMPTLY REPAIRED AND RE-VEGETATED AS NECESSARY TO PREVENT FURTHER DETERIORATION.

EROSION AND SEDIMENTATION CONTROL GENERAL NOTES:

- I. CONDUCT ALL CONSTRUCTION IN A MANNER AND SEQUENCE THAT CAUSES THE LEAST PRACTICAL DISTURBANCE OF THE PHYSICAL ENVIRONMENT, BUT IN NO CASE SHALL EXCEED 5 ACRES AT ANY ONE TIME BEFORE DISTURBED AREAS ARE STABILIZED.
- ALL EROSION AND SEDIMENTATION CONTROL MEASURES IN THE PLAN SHALL MEET THE DESIGN BASED ON NEW HAMPSHIRE STORMWATER MANUAL, VOLUMES 1-3: DATED DECEMBER 2008, PREPARED BY NHDES.
- 3. AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURRED:

• BASE COURSE GRAVELS HAVE BEEN INSTALLED

- IN AREAS TO BE PAVED. • A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED.
- · A MINIMUM OF 3" OF NON-EROSIVE MATERIAL SUCH AS STONE OR RIP RAP HAS BEEN
- EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.

INSTALLED.

- 4. ALL AREAS SHALL BE STABILIZED WITHIN 45 DAYS OF INITIAL DISTURBANCE.
- 5. SEE WINTER CONSTRUCTION NOTES IF SCHEDULE AND DATES ARE APPLICABLE.
- 6. ALL DITCHES, SWALES AND PONDS MUST BE

STABILIZED PRIOR TO DIRECTING FLOW TO THEM.

- 7. ALL GROUND AREAS OPENED UP FOR CONSTRUCTION WILL BE STABILIZED IN THE SHORTEST PRACTICAL TIME. ALL SOILS FINISH GRADED MUST BE STABILIZED WITHIN SEVENTY TWO HOURS OF DISTURBANCE
- EMPLOY TEMPORARY EROSION AND SEDIMENTATION CONTROL DEVICES AS DETAILED ON THIS PLAN AS NECESSARY UNTIL ADEQUATE STABILIZATION HAS BEEN ASSURED.
- 9. TEMPORARY & LONG TERM SEEDING: USE SEED MIXTURES, FERTILIZER, LIME AND MULCHING AS RECOMMENDED (SEE SEEDING AND STABILIZATION NOTES).
- IO. STRAW OR HAY BALE BARRIERS AND SILTATION FENCING TO BE SECURELY EMBEDDED AND STAKED AS DETAILED. WHEREVER POSSIBLE A VEGETATED STRIP OF AT LEAST TWENTY FIVE FEET IS TO BE KEPT BETWEEN SILT FENCE AND ANY EDGE OF WET AREA.
- SEEDED AREAS WILL BE FERTILIZED AND RE-SEEDED AS NECESSARY TO ENSURE VEGETATIVE ESTABLISHMENT.
- 12. SEDIMENT BASIN(S), IF REQUIRED, TO BE CHECKED AFTER EACH SIGNIFICANT RAINFALL AND CLEANED AS NEEDED TO RETAIN DESIGN CAPACITY.
- 13. STRAW BALE AND/OR SILT FENCE BARRIERS WILL BE CHECKED REGULARLY AND AFTER EACH SIGNIFICANT RAINFALL, NECESSARY REPAIRS WILL BE MADE TO CORRECT UNDERMINING OR DETERIORATION OF THE BARRIER AS WELL AS CLEANING, REMOVAL AND PROPER DISPOSAL OF TRAPPED SEDIMENT.
- 14. TREATMENT SWALES WILL BE CHECKED WEEKLY AND REPAIRED WHEN NECESSARY UNTIL ADEQUATE VEGETATIVE COVER HAS BEEN ESTABLISHED.
- 15. THE PROJECT IS TO BE MANAGED IN A MANNER THAT MEETS THE REQUIREMENTS AND INTENT OF RSA 430:53 AND CHAPTER AGR 3800 RELATIVE TO INVASIVE SPECIES.
- 16. TEMPORARY WATER DIVERSION (SWALES, BASINS, ETC.) MUST BE USED AS NECESSARY UNTIL AREAS ARE STABILIZED. SEEDING AND STABILIZATION FOR
- LOAMED SITE: FOR TEMPORARY & LONG TERM SEEDINGS (BY SEPTEMBER 15 OF THE SAME YEAR OF DISTURBANCE) USE AGWAY'S SOIL

CONSERVATION GRASS SEED OR EQUAL.

- 2. COMPONENTS: ANNUAL RYE GRASS, PERENNIAL RYE GRASS, WHITE CLOVER, 2 FESCUES, SEED AT A RATE OF 100 POUNDS PER ACRE.
- FERTILIZER & LIME: NITROGEN (N) 50 LBS/ACRE, PHOSPHATE (P205) 100 LBS/ACRE, POTASH (K20) 100
- LBS/ACRE, LIME 2000 LBS/ACRE. MULCH: HAY OR STRAW 1.5-2 TONS/ACRE.
- GRADING AND SHAPING: SLOPES SHALL NOT BE STEEPER THAN 2:1; 3:1 SLOPES OR FLATTER ARE PREFERRED. WHERE MOWING WILL BE DONE, 3:1 SLOPES OR FLATTER ARE RECOMMENDED.
- SEED BED PREPARATION - SURFACE AND SEEPAGE WATER SHOULD BE DRAINED OR DIVERTED FROM THE SITE TO PREVENT DROWNING OR WINTER KILLING OF THE
- STONES LARGER THAN 4 INCHES AND TRASH SHOULD BE REMOVED BECAUSE THEY INTERFERE WITH SEEDING AND FUTURE MAINTENANCE OF THE AREA. WHERE FEASIBLE, THE SOIL SHOULD BE TILLED TO A DEPTH OF ABOUT 4 INCHES TO PREPARE A SEEDBED AND MIX FERTILIZER AND LIME INTO THE SOIL. THE SEEDBED SHOULD BE LEFT IN A REASONABLY FIRM AND SMOOTH CONDITION. THE LAST TILLAGE OPERATION SHOULD BE PERFORMED ACROSS THE SLOPE

WHEREVER PRACTICAL.

STABILIZATION CONSTRUCTION ENTRANCE SPECIFICATIONS:

- STONE FOR A STABILIZED CONSTRUCTION ENTRANCE SHALL BE 3 INCH STONE (MINIMUM), RECLAIMED STONE, OR RECYCLED CONCRETE **EQUIVALENT**
- 2. THE LENGTH OF THE STABILIZED ENTRANCE SHALL NOT BE LESS THAN 75 FEET (OR 50 FEET WITH A 3 TO 6 INCH MOUNTABLE BERM), EXCEPT FOR A SINGLE RESIDENTIAL LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY.
- 3. THE THICKNESS OF THE STONE FOR THE STABILIZATION ENTRANCE SHALL NOT BE LESS THAN 6 INCHES.

4. THE WIDTH OF THE ENTRANCE SHALL NOT BE

LESS THAN THE FULL WIDTH OF THE ENTRANCE

- WHERE INGRESS OR EGRESS OCCURS OR IO FEET, WHICH EVER IS GREATER. 5. GEOTEXTILE FILTER CLOTH SHALL BE PLACED
- OVER THE ENTIRE AREA PRIOR TO PLACING THE STONE. FILTER CLOTH IS NOT REQUIRED FOR A SINGLE FAMILY RESIDENCE LOT.
- 6. ALL SURFACE WATER THAT IS FLOWING TO OR DIVERTED TOWARDS THE CONSTRUCTION ENTRANCE SHALL BE PIPED BENEATH THE ENTRANCE. IF PIPING IS IMPRACTICAL, A BERM WITH 5:1 SLOPES THAT CAN BE CROSSED BY VEHICLES MAY BE SUBSTITUTED FOR THE PIPE.
- THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING OF ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAF SEDIMENT. ALL SEDIMENT SPILLED, WASHED, OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED PROMPTLY.
- 8. WHEELS SHALL BE CLEANED TO REMOVE MUD PRIOR TO ENTRANCE ONTO PUBLIC RIGHTS-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.

FILTREXX LAND IMPROVEMENT SYSTEMS INSPECTION & MAINTENANCE:

- . CONSULT FILTREXX SWPP CUT SHEETS FOR ALL FILTREXX PRODUCTS PRIOR TO INSTALLATION AND FOR MAINTENANCE GUIDELINES. HTTP://WWW.FILTREXX.COM/DESIGN_CUT_SHEETS.HTM
- 2. ROUTINE INSPECTION SHOULD BE CONDUCTED WITHIN 24 HRS OF A RUNOFF EVENT OR AS DESIGNATED BY THE REGULATING AUTHORITY UNITS SHOULD BE REGULARLY INSPECTED TO MAKE SURE THEY MAINTAIN THEIR SHAPE AND ARE PRODUCING ADEQUATE HYDRAULIC FLOW-THROUGH, DITCH/CHANNEL EROSION CONTROL, AND SEDIMENT REMOVAL.
- 3. IF PONDING BECOMES EXCESSIVE, ADDITIONAL CHECK DAMS, LEVEL SPREADERS, OR SEDIMENT CONTROL UNITS FOR SEDIMENT REMOVAL MAY
- 4. SEDIMENT ACCUMULATION SHOULD BE REMOVED ONCE IT REACHES THE HEIGHT OF THE CHECK DAM OR UNIT. ALTERNATIVELY, ANOTHER UNIT MAY BE INSTALLED SLIGHTLY UPSLOPE, ON TOP OF THE EXISTING ONE. THIS PROCESS IS NOT CONSIDERED A SOIL DISTURBING ACTIVITY.
- STORM DEBRIS ACCUMULATION BEHIND CHECK DAMS, LEVEL SPREADER, SEDIMENT CONTROL UNIT, ETC. SHOULD NEVER BE HIGHER THAN THE SIDES OF THE CHECK DAM/UNIT. STORM RUNOFF OVERFLOW SHALL MAINTAIN THE UNITS IN A FUNCTIONAL CONDITION AT ALL TIMES AND IT SHALL BE ROUTINELY INSPECTED.
- 6. IF A UNIT HAS BEEN DAMAGED, IT SHALL BE REPAIRED, OR REPLACED IF BEYOND REPAIR.
- 7. THE CONTRACTOR SHALL REMOVE SEDIMENT AT THE BASE OF THE UPSLOPE SIDE OF UNITS WHEN ACCUMULATION HAS REACHED 1/2 OF THE EFFECTIVE HEIGHT OF THE SOXX, OR AS DIRECTED BY THE ENGINEER.
- 8. AS AN ALTERNATIVE, ANOTHER SOXX UNIT MAY BE INSTALLED ADJACENT AND PARALLEL TO THE UPSLOPE SIDE OF THE ORIGINAL TO INCREASE SEDIMENT STORAGE CAPACITY. SOXX SEDIMENT BACKUP IN CENTER OF THE DITCH/CHANNEL SHALL REMAIN LOWER THAN THE
- 9. IF SOXX UNIT BECOMES CLOGGED WITH DEBRIS AND SEDIMENT, IMMEDIATE REMOVAL OF DEBRIS AND SEDIMENT SHOULD BE CONDUCTED TO ASSURE PROPER DRAINAGE AND WATER FLOW THROUGH THE DITCH OR CHANNEL. STORM RUNOFF OVERFLOW OF THE SOXX UNIT IS ACCEPTABLE.
- IO. SOXX UNITS SHALL BE MAINTAINED UNTIL DISTURBED AREA AROUND THE DEVICE HAS BEEN PERMANENTLY STABILIZED AND CONSTRUCTION ACTIVITY HAS CEASED.
- II. THE FILTERMEDIATM MAY BE DISPERSED ON SITE ONCE DISTURBED AREA HAS PERMANENTLY STABILIZED, CONSTRUCTION ACTIVITY CEASED. OR DETERMINED BY THE ENGINEER.
- 12. PERMANENT VEGETATED FILTER STRIPS WILL BE LEFT INTACT.

SECTION I- GENERAL (POROUS ASPHALT PAVEMENTS)

- SUBMITTALS A. THE CONTRACTOR SHALL SUBMIT TO THE SUPERVISORY ENGINEER THE PROPOSED SOURCE AND QUALIFICATIONS OF THE PROPOSED SOURCE(S) OF THE HOT MIX ASPHALT AT LEAST 14 DAYS IN ADVANCE OF ANTICIPATED PAVING DATE
- B. THE CONTRACTOR SHALL SUBMIT TO THE SUPERVISORY ENGINEER THE PROPOSED INSTALLER QUALIFICATIONS AT LEAST 14 DAYS IN ADVANCE OF ANTICIPATED PAVING DATE. C. THE CONTRACTOR SHALL SUBMIT TO THE
- SUPERVISORY ENGINEER THE CONTRACTOR'S PROPOSED CONSTRUCTION PHASING PLAN AT LEAST 14 DAYS IN ADVANCE OF MOBILIZING TO THE SITE FOR CONSTRUCTION. UPDATES TO THE CONSTRUCTION PHASING PLAN SHALL BE PROVIDED TO THE SUPERVISORY ENGINEER AT LEAST 48 HOURS IN ADVANCE OF THE PROPOSED. THE CONSTRUCTION PHASING PLAN SHALL CONTAIN THE ELEMENTS AS DETAILED WITHIN THIS SECTION AND DRAWINGS
- D. THE CONTRACTOR SHALL SUBMIT TO THE SUPERVISORY ENGINEER THE PROPOSED THIRD PARTY QUALITY CONTROL FIRM TO CONDUCT THIRD PARTY QUALITY CONTROL OF THE ASPHALT HOT MIX PLANT PRODUCTION AT LEAST 14 DAYS IN ADVANCE OF ANTICIPATED PAVING DATE.
- E. THE CONTRACTOR SHALL SUBMIT TO THE SUPERVISORY ENGINEER THE QUALITY CONTROL RESULTS AND JOB MIX FORMULA FOR THE POROUS ASPHALT MATERIAL AT LEAST 14 DAYS IN ADVANCE OF THE ANTICIPATED PAVING DATE.
- 1.02 QUALIFICATIONS A. THE POROUS ASPHALT SHALL BE SUPPLIED FROM A HOT MIX MATERIAL PROVIDER THAT HAS THE FOLLOWING MINIMUM QUALIFICATIONS: I. SHALL HAVE SUCCESSFULLY PRODUCED A MINIMUM OF THREE (3) POROUS ASPHALT PAVING JOBS IN THE PAST FIVE (5) YEARS.
- 2. CAPABLE OF PRODUCING POROUS ASPHALT WITH A PG76-28 BINDER; UNDER NO CIRCUMSTANCES IS A PGAB 64-28 ACCEPTABLE IN REPLACE OF PG 76-28 3 CAPABLE OF CONDUCTING THE MATERIALS TESTING FOR QUALITY CONTROL AS DOCUMENTED IN SECTION IV PART 4, TABLE
- 3, TABLE 4, TABLE 5; 4. CAPABLE OF PROVIDING MATERIAL CFRTIFICATES SIGNED BY THE PLANTS' AUTHORIZED REPRESENTATIVE; AND 5. CAPABLE OF PROVIDING THE MOST RECENT ANNUAL PLANT SCALE TESTING
- DOCUMENTATION. B. THE POROUS ASPHALT INSTALLER SHALL HAVE THE FOLLOWING MINIMUM QUALIFICATIONS: I. SHALL HAVE SUCCESSFULLY COMPLETED A MINIMUM OF THREE (3) POROUS ASPHALT PAVING JOBS IN THE PAST FIVE (5) YEARS.
- 2. PROVIDE A SITE SUPERINTENDENT THAT WILL BE ON-SITE DURING THE PROJECT THAT HAS SUCCESSFULLY COMPLETED A MINIMUM OF THREE (3) POROUS ASPHALT PAVING JOBS IN THE PAST FIVE (5) YEARS.
- 103 TRANSPORTATION AND SHIPPING A. POROUS ASPHALT MATERIALS SHALL BE TRANSPORTED TO THE SITE SUCH THAT THE TEMPERATURE OF THE ASPHALT AT THE TIME OF DISCHARGE FROM THE HAUL VEHICLE SHALL BE AS PER SECTION IV - 3.05 E TEMPERATURE REQUIREMENTS, UNLESS OTHERWISE SPECIFIED BY THE HOT MIX PLANT AND APPROVED BY THE SUPERVISORY ENGINEER.
- 104 ENVIRONMENTAL CONDITIONS A. THE ASPHALT PAVING CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING ANY EROSION AND SEDIMENT CONTROLS THAT ARE DAMAGED
- FROM PAVING ACTIVITIES. B. WASTE GENERATED DURING ASPHALT PAVING SHALL BE PROPERLY DISPOSED OF ACCORDING TO THE PROJECT SPECIFICATIONS AND LOCAL, STATE, AND FEDERAL
- REGULATIONS C. ASPHALT HAUL TRUCKS SHALL EXIT THE SITE THROUGH THE DESIGNATED STABILIZED CONSTRUCTION ENTRANCE TO PREVENT TRACK
- 1.05 SCHEDULE FOR CONSTRUCTION DATES A. AFTER MAY 15 OR THE DATE OF ASPHALT PLANT OPENING UNTIL DECEMBER I OR THE DATE OF ASPHALT PLANT CLOSURE OR PER APPROVAL OF SUPERVISING ENGINEER.
- 1.06 REQUIREMENTS FOR CONSTRUCTION PHASING A. CONSTRUCTION PHASING, SEQUENCING AND ENGINEERING OVERSIGHT IS REQUIRED TO ENSURE THE SUCCESSFUL PRODUCTION, INSTALLATION, AND LONG-TERM PERFORMANCE OF POROUS PAVEMENT SYSTEMS. PROPER COORDINATION OF THESE PROCEDURES WITH THE CONTRACTOR AND INSPECTION OF THE PAVEMENT SUBGRADE DURING CONSTRUCTION IS CRITICAL TO PROVIDE ACCESS AND PREVENT DAMAGE TO POROUS PAVEMENT SYSTEM COMPONENTS, TEMPORARY CONSTRUCTION METHODS AND PHASING CONSIDERATIONS ACCOUNT FOR THE NECESSARY USE OF LARGE CONSTRUCTION EQUIPMENT OVER THE POROUS PAVEMENT LAYERS WHILE MAINTAINING ITS STRUCTURAL INTEGRITY AND INFILTRATIVE CAPACITY. THE CONTRACTOR'S CONSTRUCTION PHASING SEQUENCE PLAN SHALL INCLUDE PROTECTIVE AND CORRECTIVE ACTIONS

DETAILED BELOW FOR EXPECTED IMPACTS

FROM CONSTRUCTION ACTIVITIES. B. THE FOLLOWING CONSTRUCTION PHASING IS INTENDED AS A GUIDE. PHASING MUST BE PLANNED SUCH THAT NO CONSTRUCTION TRAFFIC IS PERMITTED ON A COMPLETED POROUS ASPHALT WEARING COURSE SURFACE AREA, CONSTRUCTION TRAFFIC IS PERMITTED ON THE TEMPORARY CONSTRUCTION ROAD, SUBGRADE AND ON THE SUBBASE DURING PREPARATION. THE USE OF A TEMPORARY POROUS ASPHALT CONSTRUCTION ROAD SHOULD ENABLE CONSTRUCTION TRAFFIC TO PROCEED WITH PHASED COMPLETION AND CLOSURE OF AREAS. INFILTRATION BEDS WILL NEED TO BE PROTECTED FROM EROSION AND SEDIMENTATION RUN-ON. IT IS RECOMMENDED THAT AREAS ARE COMPLETED INCREMENTALLY UNTIL PAVING IS COMPLETED. THE PHASING PLAN WILL BE ADAPTED BASED ON FEEDBACK WITH THE CLIENT, THE SUPERVISORY ENGINEER, AND THE CONTRACTOR.

- C. THE CONTRACTOR SHALL INCLUDE THE ELEMENTS OF THIS PHASING IN THE CONTRACTOR'S CONSTRUCTION PHASING PLAN. CONTRACTOR SUBMITTALS AND APPROVALS 2. HOST A PRE-CONSTRUCTION MEETING AT THE
- 3. EROSION AND SEDIMENTATION CONTROL BMPS ESTABLISHED INCLUDING SEDIMENTATION POND AT DOWNHILL END OF SITE POROUS PAVEMENT RESERVOIRS MAY BE USED FOR TEMPORARY SEDIMENTATION PONDS. ACCUMULATED FINES SHALL BE REMOVED PRIOR TO PLACEMENT OF AGGREGATE AND APPROVED BY THE SUPERVISING ENGINEER. 4. ROUGH GRADE SITE (CUT/FILL)
- 5. FINE GRADE SUBGRADE 6. PERFORM TOPOGRAPHICAL SURVEY OF SUBGRADE
- 7. SUPERVISORY ENGINEER TO INSPECT SUBGRADE AND PERFORM INFILTRATION TESTS TO VERIFY SUITABILITY OF SUBGRADE FROM COMPACTION DURING CONSTRUCTION OR WHERE EROSION HAS CAUSED ACCUMULATION OF FINE MATERIALS. REMORK MATERIALS THAT DO NOT MEET INFILTRATION REQUIREMENTS PER THE DRAWINGS AND SPECIFICATIONS. THESE MATERIALS SHALL BE REMOVED AND/OR SCARIFIED TO A MINIMUM DEPTH OF 6 INCHES, AND RETESTED FOR COMPACTION AND INFILTRATION AS PER
- SPECIFICATIONS. 8. INSTALL GEOTEXTILE VERTICAL BARRIERS PLACED ALONG PERIMETER OF POROUS PAVEMENT PARKING AREA PER THE
- DRAWINGS 9. INSTALL CAPILLARY BARRIER AND GEOTEXTILE INTERNAL GRADE CONTROLS IO. PLACE UTILITIES LINES OVER THE GRADED CAPILLARY BARRIER LAYER
- II. PLACE AND COMPACT FILTER COURSE PER THIS SECTION 12. SUPERVISORY ENGINEER TO INSPECT FILTER COURSE AND PERFORM INFILTRATION TESTS TO VERIFY SUITABILITY OF COMPACTION AND INFILTRATION PER THIS SECTION.
- 13. PLACE AND GRADE CHOKER COARSE 14.PLACE AND COMPACT POROUS ASPHALT BINDER COURSE.
- 1.07 PLACEMENT OF TEMPORARY ROAD OF POROUS ASPHALT BINDER COURSE (OPTIONAL) A. INSTALL AT THICKNESS INDICATED ON DRAWINGS (IN PLACE) LAYER OF BINDER COURSE PER THIS SECTION.
- B. INSTALL FRAME, GRATES, AND LANDSCAPING. SPECIAL CARE IS TO BE TAKEN TO PROTECT FRESH BINDER COURSE. C. ALL TRUCKS (INCLUDING CONCRETE TRUCKS)
- WILL BE STOPPED PRIOR TO ENTERING THE SITE AND INSTRUCTED AS TO SPECIAL CONCERNS FOR PAVEMENT DURABILITY D. A WASHOUT AREA FOR ALL CONCRETE TRUCKS SHALL BE DESIGNATED OUTSIDE OF POROUS PAVEMENT AREA ON THE CONTRACTOR'S
- EROSION AND SEDIMENT CONTROL PLAN. E. POROUS PAVEMENT SURFACE SHALL BE PROTECTED ON HOT DAYS DURING THE PAVEMENT CURE PERIOD (2-3 DAYS). SURFACE TEMPERATURES CAN QUICKLY REACH OVER
- 145°F IN DIRECT SUN. F. A TEMPERATURE GUN SHALL BE AVAILABLE ONSITE TO ASSESS PAVEMENT SURFACE TEMPERATURES. PAVEMENT TEMPERATURES GREATER THAN 100°F SHOULD BE OBSERVED CAREFULLY FOR PAVEMENT DURABILITY. AS NEEDED, COOLING OF PAVEMENT SURFACE BY APPLICATION OF WATER FROM A WATER TRUCK SHOULD OCCUR WHEN HEAVY VEHICULAR TRAFFIC IS EXPECTED SUCH AS CONCRETE TRUCKS FOR DRY WELL FRAME AND GRATE INSTALLATION. IN THE EVENT THIS IS INEFFECTIVE FOR COOLING AND PAVEMENT DEFORMATION IS STILL OBSERVED, THE USE OF 3/4" PLYWOOD UNDER LARGE VEHICLE WHEELS
- MAY BE REQUIRED. G. TRUCKS AND OTHER CONSTRUCTION TRAFFIC WILL NOT BE ALLOWED TO ACCESS THE SITE WHILE THE PAVEMENT IS EXCESSIVELY HOT >130°F OR IF UNACCEPTABLE DAMAGE IS OBSERVED. COSMETIC DAMAGE TO BINDER COURSE IS ACCEPTABLE NOT INCLUDING LOSS
- OF INFILTRATION CAPACITY H. NO STOCKPILING OF MATERIALS (E.G. SOIL, STONE, LANDSCAPING MATERIALS) WILL BE ALLOWED ON POROUS PAVEMENTS.
- I. MATERIALS EXCAVATED FOR FINISH WORKS SHALL BE PLACED OUTSIDE OF POROUS PAVEMENT AREAS. J. VACUUMING THROUGHOUT CONSTRUCTION MAY BE NECESSARY FOR SURROUNDING PAVED
- AREAS TO PREVENT RUN-ON OR TRACKING ONTO POROUS PAVEMENTS. FREQUENCY SHALL BE ADJUSTED AS NEEDED. K. REPEAT PHASE I AND 2 INCREMENTALLY UNTIL
- FULL PAVING IS COMPLETED. SECTION II-PAVEMENT SUBGRADE (POROUS ASPHALT PAVEMENTS) PART I EXECUTION
- LOI EXAMINATION A. EXAMINE SPACES TO BE FILLED BEFOREHAND AND REMOVE ALL UNSUITABLE MATERIALS AND DEBRIS INCLUDING SHEETING, FORMS, TRASH, STUMPS, PLANT LIFE, ETC.
- INSPECT BACKFILL AND FILL MATERIALS BEFOREHAND AND REMOVE ALL ROOTS, VEGETATION, ORGANIC MATTER, OR OTHER FOREIGN DEBRIS. STONES LARGER THAN 12 INCHES IN ANY DIMENSION SHALL ALSO BE REMOVED OR BROKEN INTO SMALLER PIECES. NO BACKFILL OR FILL MATERIAL SHALL BE
- PLACED ON FROZEN GROUND NOR SHALL THE MATERIAL ITSELF BE FROZEN OR CONTAIN FROZEN SOIL FRAGMENTS D. SPACES TO BE FILLED SHALL BE FREE FROM STANDING WATER SO THAT PLACEMENT AND COMPACTION OF THE FILL MATERIALS CAN BE ACCOMPLISHED IN "DRY" CONDITIONS.
- ALL UNDERGROUND UTILITY INSTALLATIONS, INCLUDING CULVERTS, SHALL BE COMPLETED. BACKFILLED AND COMPACTED PRIOR TO COMPLETION OF SUBGRADE. VERIFY THAT TRAFFIC CONTROLS AND EROSION AND SEDIMENT CONTROLS ARE IN

PLACE.

- 1.02 PREPARATION A. TEMPORARY EROSION AND SEDIMENT
- CONTROLS SHALL BE INSTALLED PRIOR TO CONSTRUCTION OF SUBGRADE. B. TAKE ANY OTHER NECESSARY STEPS TO PREVENT SEDIMENT FROM WASHING INTO INFILTRATION BEDS DURING CONSTRUCTION. WHEN THE SITE IS FULLY STABILIZED. TEMPORARY SEDIMENT CONTROL DEVICES
- SHALL BE REMOVED. C. TEMPORARY DRAINS AND DITCHES SHALL BE CONSTRUCTED AS NECESSARY TO REMOVE WATER FROM THE SUBGRADE AREA.
- I. TEMPORARY DRAINAGE OPENINGS IN EXISTING CATCH BASINS MAY BE MADE IN A MANNER ACCEPTABLE TO THE ENGINEER. SUCH OPENINGS TO BE REPAIRED TO THE SATISFACTION OF THE ENGINEER. 2. CONTRACTOR TO PREVENT THE ENTRANCE
- OF DEBRIS, STONES AND SILT FROM ENTERING DRAINAGE SYSTEMS, INCLUDING THE USE OF BALES OF HAY, SCREENS AND OTHER DESILTING METHODS D. BACKFILLED AREAS SHALL BE RETESTED AT THE DISCRETION OF THE ENGINEER
- SUBGRADE. F. IN MOST INSTANCES TRAVEL UPON SUBGRADE IS UNAYOIDABLE, AND A CAREFUL ASSESSMENT OF DEGREE OF SUBGRADE COMPACTION IS

MINIMIZE TRAFFIC AND COMPACTION UPON

- NEEDED. TILLING AND OR REMOVAL OF COMPACTED SUBGRADE MAY BE NEEDED. G. SUBGRADE COMPACTED DURING EXCAVATION OR WHERE EROSION HAS CAUSED ACCUMULATION OF FINE MATERIALS, THIS MATERIAL SHALL BE REMOVED AND/OR
- SCARIFIED TO A MINIMUM DEPTH OF 6 INCHES. H. PRIOR TO PLACEMENT OF THE AGGREGATE RESERVOIR (AGGREGATE BASE COURSE), THE INFILTRATION RATE OF THE SUBGRADE SHALL BE DETERMINED BY ASTM D3385 OR APPROVED ALTERNATE AT THE DISCRETION OF THE ENGINEER. THE INFILTRATION RATE SHALL BE NO LESS 5-30 FT/DAY OR 50% OF THE HYDRAULIC CONDUCTIVITY (D2434) AT 95% STANDARD PROCTOR COMPACTION.
- SEE TABLE 2 FOR COMPACTION AND INFILTRATION REQUIREMENTS. J. THE DENSITY OF SUBGRADE COURSES SHALL BE DETERMINED BY AASHTO T 191 (SAND-CONE METHOD), AASHTO T 204 (DRIVE CYLINDER METHOD), OR AASHTO T 238 (NUCLEAR METHODS), OR OTHER APPROVED METHODS AT
- THE DISCRETION OF THE ENGINEER. K. UNSUITABLE MATERIALS SHALL BE REMOVED AND REMORKED TO THE SATISFACTION OF THE ONSITE ENGINEER.
- L. UPON COMPLETION OF SUBGRADE WORK, THE ENGINEER SHALL BE NOTIFIED AND SHALL INSPECT AT HIS/HER DISCRETION BEFORE PROCEEDING WITH THE POROUS MEDIA BED INSTALLATION.
- 1.03 FIELD QUALITY CONTROL A. FOR COMPACTION REQUIREMENTS SEE TABLE 2. B. TOLERANCES - THE FINAL SUBGRADE SURFACE SHALL NOT VARY MORE THAN ±1/2 INCH FROM THE DESIGN GRADE ELEVATION AT ANY LOCATION, PARALLEL TO THE FINAL ROAD
- SURFACE AS DEFINED BY THE TOTAL ROADWAY THICKNESS. C. PROOF ROLLED - PRIOR TO THE PLACEMENT OF THE NEXT PAVEMENT COURSE, THE SUBGRADE SURFACE SHALL BE PROOF ROLLED TO LOCATE AREAS OF INADEQUATE COMPACTION OR DEFECTIONS OR SOFT OR RUTTING AREAS REQUIRING UNDERCUTTING, WITH 8- TO IO-TON PNEUMATIC TIRE COMPACTORS.
- I. AREAS OF INADEQUATE COMPACTION TO BE RECOMPACTED. 2. IF ADDITIONAL ROLLING DOES NOT CORRECT AN AREA OF UNSTABLE CONDITION, THEN THIS AREA AND SOFT OR RUTTED AREAS SHALL BE REMOVED AND
- REPLACED WITH SELECT ON-SITE MATERIAL AND COMPACTED. 3. WHERE NO SUITABLE ON-SITE MATERIAL IS AVAILABLE, GRANULAR MATERIALS SHALL BE INSTALLED AND COMPACTED; AREAS INACCESSIBLE TO ROLLERS TO BE COMPACTED BY MECHANICAL METHODS.

SECTION III: AGGREGATE BASE COURSE (POROUS ASPHALT PAVEMENTS)

PART I EXECUTION

- I.OI EXAMINATION A. VERIFY PAVEMENT SUBGRADE HAS BEEN ACCEPTED FOR PLACEMENT OF AGGREGATE BASE COURSE I. GRADIENTS, CROWNS AND ELEVATIONS ARE
- CORRECT. 2. SUBGRADE IS DRY 3. PRIOR TO PLACEMENT OF THE AGGREGATE, THE INFILTRATION RATE OF THE SUBGRADE SHALL BE DETERMINED BY ASTM D3385 OR APPROVED ALTERNATE AT THE DISCRETION OF THE ENGINEER. THE INFILTRATION RATE SHALL BE NO LESS 5-30 FT/DAY OR 50% OF THE HYDRAULIC CONDUCTIVITY (D2434)

AT 95% STANDARD PROCTOR COMPACTION.

- B. VERIFY THAT TRAFFIC CONTROLS ARE IN PLACE.
- 1.02 EDGE LINER INSTALLATION A. EDGE GEOTEXTILE OR PVC LINER SHALL BE PLACED IMMEDIATELY AFTER APPROVAL OF SUBGRADE PREPARATION B. THE EDGE LINER IS TO BE PLACED ALONG THE
- TRANSITION ELEMENT TO THE DIMENSIONS AND LOCATIONS AS SHOWN WITHIN THE CONTRACT THE LINER IS TO BE PLACED BEHIND THE CURB

ENTIRE PERIMETER OF THE VERTICAL WALLS

OF BOTH SIDES OF THE EXCAVATION AND

LOCATED BEHIND THE CURB, SIDEWALK, OR

- OR TRANSITION WITHIN CONCRETE FOOTING. D. THE EDGE LINER SHOULD TEMPORARILY BE STAKED VERTICALLY AT 12-18" ABOVE THE SUBGRADE, PRIOR TO PLACEMENT BEHIND CURB, TO FUNCTION AS EROSION CONTROL MEASURE TO PREVENT FINES FROM WASHING
- INTO RESERVOIR BASE. . PENETRATIONS TO THE PVC LINER SHALL BE WRAPPED WITH A STAINLESS STEEL PIPE CLAMP, SEALED BY HEAT-SHRINK, OR SIMILAR METHOD TO ACHIEVE LOW PRESSURE WATER TIGHT SEAL OR APPROVED EQUAL TO PREVENT THE MIGRATION OF SEDIMENT ACROSS THE

PENETRATION.

- F. INTERNAL GRADE PVC LINER GRADE CONTROL TO BE PLACED EVERY 12" OF GRADE LOSS AT EQUAL ELEVATION ALONG THE CONTOUR. THE INTERNAL GRADE CONTROL ARE TO CONTAIN THE FLOW ON SLOPE WITHIN THE PAVEMENT RESERVOIR AND MUST BE KEYED INTO EDGE PVC LINER AND CONTAIN THE RESERVOIR BED AND SUBGRADE.
- B. THE INTERNAL GRADE CONTROL PVC LINER IS TO BE PLACED ALONG AN EQUAL ELEVATION CONTOUR AS PER THE DIMENSIONS AND LOCATIONS AS SHOWN WITHIN THE CONTRACT DRAWINGS.
- C. PENETRATIONS FROM UTILITIES TO THE PVC LINER ARE TO BE MINIMIZED AND LOCATED BENEATH THE PVC LINER IF POSSIBLE. D. UTILITY PIPING WITHIN THE ROADBED SHALL BE

WATERTIGHT AND SEALED WITH FOAM, CAULKING, OR

- OTHER SUITABLE METHOD. E. ALL UTILITY TRENCHES THAT INTERSECT OR TRAVEL BELOW THE PAVEMENT SUBBASE SHALL HAVE CONSIDERATIONS TO PREVENT SOIL PIPING AND INFILTRATION AND INFLOW. THIS MAY INCLUDE
- SEEPAGE COLLAR, COVER WITH LINER, OR OTHER METHOD APPROVED BY ENGINEER. F. IN AREAS WHERE THE LINER IS NOT CONTINUOUS, A 12-INCH OVERLAP IS REQUIRED.
- 1.03 FILTER COURSE PREPARATION A. RESERVOIR COURSE AND CAPILLARY BARRIER AGGREGATE SHALL BE PLACED IMMEDIATELY AFTER APPROVAL OF SUBGRADE PREPARATION AND INSTALLATION OF EDGE GEOTEXTILE. ANY ACCUMULATION OF DEBRIS OR SEDIMENT WHICH HAS TAKEN PLACE AFTER APPROVAL OF SUBGRADE SHALL BE REMOVED PRIOR TO INSTALLATION OF GEOTEXTILE AT NO EXTRA COST TO THE OWNER. B. SEE TABLE I FOR SPECIFICATIONS FOR FILTER
- COURSE AND RESERVOIR COURSE / CAPILLARY C. SEE TABLE 2 FOR COMPACTION AND INFILTRATION
- REQUIREMENTS OF SUBBASE INSTALL FILTER COURSE AGGREGATE IN 12-INCH MAXIMUM LIFTS TO 95 TO 98% STANDARD PROCTOR COMPACTION (ASTM D698 / AASHTO T99), INSTALL AGGREGATE TO GRADES INDICATED ON THE
- DRAWINGS E. THE INFILTRATION RATE OF THE FILTER COURSE SHALL BE DETERMINED BY ASTM D3385 OR APPROVED ALTERNATE AT THE DISCRETION OF THE SUPERVISING ENGINEER. THE INFILTRATION RATE SHALL BE NO LESS 5-30 FT/DAY OR 50% OF THE HYDRAULIC CONDUCTIVITY (D2434) AT 95%
- STANDARD PROCTOR COMPACTION. F. THE DENSITY OF FILTER COURSE SHALL BE DETERMINED BY AASHTO T 191 (SAND-CONE METHOD), AASHTO T 204 (DRIVE CYLINDER METHOD), OR AASHTO T 238 (NUCLEAR METHODS), OR OTHER APPROVED METHODS AT THE DISCRETION OF THE SUPERVISING ENGINEER.
- G. VIBRATORY COMPACTION SHALL BE PERFORMED USING TWO-AXLE TANDEM ROLLERS WITH A GROSS MASS (WEIGHT) OF NOT LESS THAN 5 METRIC TONS (5.5 TONS) AND NOT MORE THAN 10 METRIC TONS (12 TONS) AND SHALL BE CAPABLE OF PROVIDING A MINIMUM COMPACTIVE EFFORT OF 44 KN/M (250 POUNDS PER INCH) OF WIDTH OF THE DRIVE ROLL. ALL ROLLS SHALL BE AT LEAST I M (42 INCHES) IN
- H. COMPACTION OF SUBGRADE COURSE MATERIAL SHALL BE DONE WITH A METHOD AND ADEQUATE WATER TO MEET THE REQUIREMENTS, ROLLING AND SHAPING SHALL CONTINUE UNTIL THE REQUIRED DENSITY IS ATTAINED. WATER SHALL BE UNIFORMLY APPLIED OVER THE SUBBASE COURSE MATERIALS DURING COMPACTION IN THE AMOUNT NECESSARY FOR PROPER CONSOLIDATION.
- 1.04 POROUS AGGREGATE SUBBASE INSTALLATION A. RESERVOIR BED AGGREGATE SHALL BE PLACED IMMEDIATELY AFTER APPROVAL OF SUBGRADE PREPARATION AND INSTALLATION OF EDGE PVC LINER, ANY ACCUMULATION OF DEBRIS OR SEDIMENT WHICH HAS TAKEN PLACE AFTER APPROVAL OF SUBGRADE SHALL BE REMOVED PRIOR TO INSTALLATION OF PVC LINER AT NO EXTRA COST TO

B. SEE TABLE 2 FOR COMPACTION AND INFILTRATION

THE OWNER.

REQUIREMENTS.

- C. INSTALL RESERVOIR BED AGGREGATE IN 12-INCH MAXIMUM LIFTS TO 95 TO 98% STANDARD PROCTOR COMPACTION (ASTM D698 / AASHTO T99). INSTALL AGGREGATE TO GRADES INDICATED ON THE DRAWINGS. D. VIBRATORY COMPACTION SHALL BE PERFORMED USING TWO-AXLE TANDEM ROLLERS WITH A GROSS MASS (WEIGHT) OF NOT LESS THAN 5 METRIC TONS (5.5 TONS) AND NOT MORE THAN 10 METRIC TONS (12 TONS) AND SHALL BE CAPABLE OF PROVIDING A
- POUNDS PER INCH) OF WIDTH OF THE DRIVE ROLL. ALL ROLLS SHALL BE AT LEAST I M (42 INCHES) IN DIAMETER. E. COMPACTION OF SUBGRADE COURSE MATERIAL SHALL BE DONE WITH A METHOD AND ADEQUATE WATER TO MEET THE REQUIREMENTS. ROLLING AND SHAPING SHALL CONTINUE UNTIL THE REQUIRED DENSITY IS ATTAINED. WATER SHALL BE UNIFORMLY APPLIED OVER THE SUBBASE COURSE MATERIALS

MINIMUM COMPACTIVE EFFORT OF 44 KN/M (250

- DURING COMPACTION IN THE AMOUNT NECESSARY FOR PROPER CONSOLIDATION. F. ADD SMALL QUANTITIES OF FINE AGGREGATE TO
- COARSE AGGREGATE AS APPROPRIATE TO ASSIST COMPACTION. G. IF EXCESS WATER IS APPARENT, REMOVE AGGREGATE AND AERATE TO REDUCE MOISTURE
- H. USE MECHANICAL VIBRATING TAMPING IN AREAS INACCESSIBLE TO COMPACTION EQUIPMENT. THE ENGINEER SHALL BE NOTIFIED AND SHALL INSPECT THE LINER AND SUBBASE INFILTRATION CAPACITY AT HIS/HER DISCRETION BEFORE PROCEEDING WITH THE PLACEMENT OF SELECT ROAD
- BASE MATERIALS J. INSPECTION OF INFILTRATION CAPACITY WILL VERIFY SUITABILITY OF SUBBASE FROM COMPACTION DURING CONSTRUCTION OR WHERE EROSION HAS CAUSED ACCUMULATION OF FINE MATERIALS, IF NEEDED. COMPACTED/ACCUMULATED MATERIALS SHALL BE REMOVED AND/OR SCARIFIED TO A MINIMUM DEPTH OF 6 INCHES AND RETESTED FOR COMPACTION AND

DETAIL. INFILTRATION TRENCH MAY BE ELIMINATED IN

INTERSECT THE EXCAVATION. 1.05 PROTECTION A, IN THE EVENT THE SUBBASE IS USED FOR MAINTENANCE OF TRAFFIC OR IS DISTURBED OR LOOSENED BY ANY CAUSE, THEN PRIOR TO PLACING OF THE NEXT PAYING COURSE, THE SUBBASE SHALL

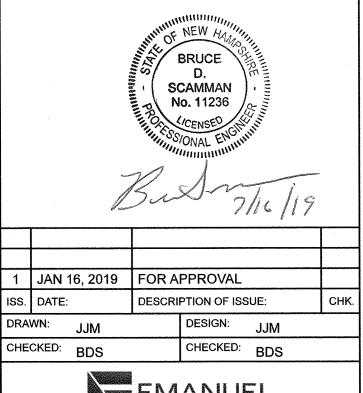
BE REGRADED AND RECOMPACTED TO ITS FINISHED

K. INSTALL INFILTRATION TRENCH PER CONSTRUCTION

THE EVENT OF SHALLOW UTILITIES THAT WILL

INFILTRATION AS PER SPECIFICATIONS.

GRADE AND SPECIFIED DENSITY.



SEAL

CLIENT:

TITLE:

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SECTION IV- POROUS ASPHALT PAVING (POROUS ASPHALT PAVEMENTS)

DR. ROBERT ROSEEN OF WATERSTONE ENGINEERING, INC. (OR EQUAL) SHALL REVIEW ALL ASPECTS OF PREPARATION, INSTALLATION, AND TESTING FOR THE POROUS PAVEMENT SECTIONS. PHONE: (603) 686-2488

I.OI SCHEDULING

- A. SCHEDULE THE PAVING OPERATIONS SUCH THAT ALL PAVING NECESSARY TO PROVIDE SAFE AND ADEQUATE MAINTENANCE AND PROTECTION OF TRAFFIC OR FOR PROTECTION OF PREVIOUSLY LAID COURSES IS COMPLETED WITHIN THE WEATHER AND SEASONAL LIMITATIONS.
- I. SUCH SCHEDULING SHALL INCLUDE EXPEDITING CONSTRUCTION OPERATIONS TO PERMIT PAYING BEFORE THE SEASONAL LIMITATIONS OR BY LIMITING THE LENGTH OF WORK TO THAT WHICH CAN BE COMPLETED BEFORE THE SEASONAL SHUTDOWN.
- 2. THE COST OF SCHEDULING AND SEQUENCING OF WORK TO CONFORM TO THE SEASONAL LIMITATIONS SHALL BE REFLECTED IN THE BID PRICES FOR THE RELATED CONTRACT ITEMS.

PART 2 PRODUCTS

- 2.01 ASPHALT CONCRETE
 A. BINDER COURSE THE PAVEMENT BINDER COURSE SHALL BE CONSTRUCTED OF THE FOLLOWING TYPE AND TO THE WIDTHS AND DEPTHS AS SHOWN ON THE DRAWINGS.
- I. THIS BINDER COURSE SHALL BE IN ACCORDANCE WITH NHDOT SPECIFICATION FOR BITUMINOUS CONCRETE.
- A. PAVEMENT MEARING COURSE (SURFACE COURSE) PAVEMENT MEARING COURSE SHALL BE CONSTRUCTED OF THE FOLLOWING TYPE AND TO THE WIDTH AND DEPTH AS SHOWN ON THE DRAWINGS.
- I. THIS MEARING COURSE SHALL BE IN ACCORDANCE WITH NHDOT SPECIFICATION FOR BITUMINOUS CONCRETE.
- A. PAINTED TRAFFIC MARKINGS CONTRACTOR
 SHALL REPLACE ALL MARKINGS IN ACCORDANCE
 WITH LOCAL, COUNTY, OR STATE SPECIFICATIONS
 (DEPENDING ON JURISDICTION).

2.02 POROUS ASPHALT

- A. THIS IS A PERFORMANCE SPECIFICATION;
 ALTERNATIVES CAN BE SUBSTITUTED IF THE MIX
 DESIGN MEETS THE MINIMUM QC PERFORMANCE
 CRITERIA FOR GRADATION, ASPHALT CONCRETE
 (AC) CONTENT, PERCENT (%) VOID SPACE, %
 DRAIN DOWN, RETAINED TENSILE STRENGTH (TSR)
 AND CANTABRO WEAR TEST AND ACCEPTED IN
 WRITING BY THE ENGINEER.
- B. POLYMER MODIFIED PERFORMANCE GRADED ASPHALT BINDER AND MIX DESIGNS
 I. POROUS ASPHALT WEARING COURSE:
- GRADATION, AC CONTENT, % VOID SPACE, % DRAIN DOWN, TSR, CANTABRO AS INDICATED IN TABLE 3. THE ASPHALT BINDER SHALL BE A TERMINAL BLENDED PG76-28 MODIFIED WITH A STYRENE BUTADIENE STYRENE.

 2. POROUS ASPHALT BINDER COURSE:
- GRADATION, AC CONTENT, % VOID SPACE, % DRAIN DOWN, TSR, CANTABRO AS INDICATED IN TABLE 3. THE ASPHALT BINDER SHALL BE A TERMINAL BLENDED PG76-28 MODIFIED WITH A STYRENE BUTADIENE STYRENE.

 3. POROUS ASPHALT MIX DESIGNS: THE
- CONTRACTOR SHALL SIZE, UNIFORMLY GRADE AND COMBINE THE AGGREGATE FRACTIONS IN PROPORTIONS THAT PROVIDE A MIXTURE MEETING THE REQUIREMENTS SPECIFIED.

PART 3 EXECUTION 3.01 PREPARATION - RESET MANHOLE FRAMES

- A. PRIOR TO PLACING WEARING (TOP) COURSE, MAKE FINAL ADJUSTMENTS OF MANHOLE FRAMES, CATCH BASIN FRAMES, VALVE BOXES AND ANY OTHER UTILITY STRUCTURES LOCATED IN THE PAVEMENT IN RELATION TO FINISHED GRADE.

 I. MANHOLE FRAMES, VALVE BOXES, ETC. TO SET
- 1/2 INCH BELOW FINISHED GRADE AND PARALLEL TO FINISHED CROWN.

 2. CATCH BASIN FRAMES TO SET I INCH BELOW
- FINISHED GRADE AND PARALLEL TO FINISHED CROWN.

 a. BEVEL SLOPE OF WEARING COURSE (FOR
- a. BEVEL SLOPE OF WEARING COURSE (FOR 6-INCH WIDTH) AROUND CATCH BASIN FRAME.

3.02 POROUS ASPHALT BINDER COURSE INSTALLATION A. TEST STRIP (OPTIONAL)

- I. AN OPTIONAL TEST STRIP SHALL BE CONDUCTED TO DETERMINE OPTIMAL COMPACTION PROCEDURES FOR THE BINDER COURSE AT A THICKNESS AS INDICATED IN THE DRAWINGS. THE TEST STRIP WILL BE CONSTRUCTED IN A PORTION OF THE SITE TO ESTABLISH AND ENSURE THE PROPER MIX DESIGN. PRODUCTION AND PLACEMENT.
- B. TWO MIX SAMPLES SHALL BE COLLECTED AT THE ASPHALT PLANT BY A 3RD PARTY QC
- TECHNICIAN DURING BINDER COURSE
 PRODUCTION FROM EACH TEST STRIP FOR
 ASPHALT CONTENT, AND GRADATION.

 4. FIELD TESTING OF INFILTRATION CAPACITY

2. THE TEST STRIP SHALL BE OVERSEEN BY THE

- FIELD TESTING OF INFILTRATION CAPACITY SHALL BE PERFORMED ON THE TEST STRIP FOR INFILTRATION BY THE ENGINEER.
- TWO CORES SHALL BE COLLECTED FROM EACH TEST STRIP AND EVALUATED FOR COMPACTION, DENSITY, AND POROSITY.
 THESE CRITERIA ONCE ESTABLISHED WILL BE APPLIED TO ALL POROUS ASPHALT
- INSTALLATIONS.

 A. CONDITIONING OF EXISTING SURFACE
 I. THE CONTRACTOR SHALL THOROUGHLY CLEAN
 THE SURFACE UPON WHICH THE BINDER COURSE
- IS TO BE PLACED OF ALL OBJECTIONABLE
 MATERIAL.

 A. PREPARATION OF AGGREGATES
 I. THE CONTRACTOR SHALL DRY AND HEAT THE
 AGGREGATES FOR THE BINDER COURSE TO
- THE REQUIRED TEMPERATURE.

 MIXING

 I. THE CONTRACTOR SHALL COMBINE THE DRIED AGGREGATE IN THE MIXER IN THE AMOUNT OF EACH FRACTION OF AGGREGATE REQUIRED TO MEET THE SPECIFICATIONS. ONCE MIXED THE BINDER COURSE SHALL BE PLACED AS SOON

AS POSSIBLE.

- E. SPREADING AND FINISHING
 I. ON AREAS WHERE IRREGULARITIES OR
 UNAVOIDABLE OBSTACLES MAKE THE USE
 OF MECHANICAL SPREADING AND FINISHING
 IMPRACTICABLE, THE CONTRACTOR SHALL
 SPREAD AND RAKE THE BINDER COURSE
- WITH HAND TOOLS TO PROVIDE THE REQUIRED COMPACTED THICKNESS.

 2. SOLVENT BASED AGENTS DEVELOPED TO STRIP ASPHALTS FROM AGGREGATES WILL
- NOT BE ALLOWED AS A RELEASE AGENT.

 3. JOINTS SHALL BE FULLY COATED WITH PGAB 76-28 JUST PRIOR TO THE PLACEMENT OF THE ADJOINING COURSE. ARES THAT BECOME CONTAMINATED OR STRIPPED OF ASPHALT COATING WILL BE RETREATED WITH ASPHALT PRIOR TO PLACING THE ADJOINING COARSE.
- A. COMPACTION
 I. THE ACTUAL METHODS AND EQUIPMENT USED
 TO COMPACT THE BINDER COURSE WILL BE
 DETERMINED DURING THE PLACEMENT AND
 COMPACTION OF THE TEST STRIP AND AS
 TABLE 2.
- 2. IMMEDIATELY AFTER THE ASPHALT TREATED PERMEABLE BASE HAS BEEN SPREAD, STRUCK OFF, AND ANY SURFACE IRREGULARITIES ADJUSTED, THE CONTRACTOR SHALL THOROUGHLY AND UNIFORMLY COMPACT THE BINDER COURSE BY ROLLING.
- 3. THE BINDER COURSE SHALL BE COMPACTED BY A MAXIMUM OF THREE COMPLETE PASSES OF A STEEL ROLLER HAVING A MINIMUM WEIGHT OF 12 TONS OPERATED IN STATIC MODE, OR 10 TONS IF EQUIPPED WITH OSCILLATORY COMPACTION AND OPERATED IN LOW FREQUENCY, LOW AMPLITUDE MODE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER, PNEUMATIC ROLLERS WILL NOT
- BE USED TO COMPACT THE BINDER COURSE.

 4. THE CONTRACTOR SHALL ROLL THE SURFACE WHEN THE MIXTURE IS IN THE PROPER CONDITION AND WHEN THE ROLLING DOES NOT CAUSE UNDUE DISPLACEMENT, CRACKING, OR SHOVING. THE CONTRACTOR SHALL PREVENT ADHESION OF THE BINDER COURSE TO THE ROLLERS OR VIBRATING COMPACTORS WITHOUT THE USE OF FUEL OIL OR OTHER PETROLEUM, OR SOLVENT BASED RELEASE AGENTS. SOLVENTS DESIGNED TO STRIP ASPHALT BINDERS FROM AGGREGATES WILL NOT BE PERMITTED AS RELEASE AGENTS ON EQUIPMENT, TOOLS OR
- BINDER COURSE SURFACES.

 5. THE CONTRACTOR SHALL IMMEDIATELY
 CORRECT ANY DISPLACEMENT OCCURRING
 AS A RESULT OF THE REVERSING OF THE
 DIRECTION OF A ROLLER OR FROM OTHER
 CAUSES TO THE SATISFACTION OF THE
- ENGINEER.

 6. ANY OPERATION THAT RESULTS IN BREAKDOWN OF THE AGGREGATE SHALL BE DISCONTINUED.
- I. AFTER A 24 HOUR CURING PERIOD OF THE BINDER COURSE, LIMITED TRAFFIC MAY BE ROUTED OVER THE BINDER COURSE SURFACE. UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER, CONSTRUCTION EQUIPMENT, AND TRAFFIC SHALL BE PROHIBITED FROM TRAVELING OVER THE BINDER COURSE SURFACE UNTIL THE ENTIRE
- PAVEMENT STRUCTURE IS IN PLACE.

 2. DAMAGE TO THE BINDER COURSE LAYER CAUSED BY CONSTRUCTION EQUIPMENT OR TRAFFIC SHALL BE REMEDIED BY COMPLETE REMOVAL REPLACEMENT OF THE DAMAGED AREA TO THE LIMITS DETERMINED BY THE ENGINEER. THERE WILL BE NO ADDITIONAL PAYMENT FOR REPAIRS, OR ASSOCIATED WORK.

3.03 PLACEMENT OF POROUS ASPHALT BINDER COURSE

- A. INSTALL THE BINDER COURSE COURSE AT A
 THICKNESS AS INDICATED IN DRAWINGS.
 B. INSTALL FRAME, GRATES, AND LANDSCAPING
- SPECIAL CARE IS TO BE TAKEN TO PROTECT FRESH BINDER COURSE.

 C. ALL TRUCKS (INCLUDING CONCRETE TRUCKS)
 WILL BE STOPPED PRIOR TO ENTERING THE
- WILL BE STOPPED PRIOR TO ENTERING THE
 SITE AND INSTRUCTED AS TO SPECIAL
 CONCERNS FOR PAVEMENT DURABILITY.

 D. A WASHOUT AREA FOR ALL CONCRETE TRUCKS
 SHALL BE DESIGNATED OUTSIDE OF POROUS
- PAVEMENT AREA ON THE CONTRACTOR'S EROSION AND SEDIMENT CONTROL PLAN.

 E. POROUS PAVEMENT SURFACE SHALL BE PROTECTED ON HOT DAYS DURING THE PAVEMENT CURE PERIOD (2-3 DAYS). SURFACE TEMPERATURES CAN QUICKLY REACH OVER 145°F IN DIRECT SUN.
- F. A TEMPERATURE GUN SHALL BE AVAILABLE ONSITE TO ASSESS PAVEMENT SURFACE TEMPERATURES. PAVEMENT TEMPERATURES GREATER THAN IOO°F SHOULD BE OBSERVED CAREFULLY FOR PAVEMENT DURABILITY. AS NEEDED, COOLING OF PAVEMENT SURFACE BY APPLICATION OF WATER FROM A WATER TRUCK SHOULD OCCUR WHEN HEAVY VEHICULAR TRAFFIC IS EXPECTED SUCH AS CONCRETE TRUCKS FOR DRY WELL FRAME AND GRATE INSTALLATION. IN THE EVENT THIS IS INEFFECTIVE FOR COOLING AND PAVEMENT DEFORMATION IS STILL OBSERVED, THE USE OF 3/4" PLYWOOD UNDER LARGE VEHICLE WHEELS MAY BE REQUIRED.
- G. TRUCKS AND OTHER CONSTRUCTION TRAFFIC WILL NOT BE ALLOWED TO ACCESS THE SITE WHILE THE PAVEMENT IS EXCESSIVELY HOT >130°F.
- H. NO STOCKPILING OF MATERIALS (E.G. SOIL, STONE, LANDSCAPING MATERIALS) WILL BE
- ALLOWED ON POROUS PAVEMENTS.

 I. MATERIALS EXCAVATED FOR FINISH WORKS SHALL BE PLACED OUTSIDE OF POROUS PAVEMENT AREAS.
- J. VACUMING THROUGHOUT CONSTRUCTION MAY BE NECESSARY FOR SURROUNDING PAYED AREAS TO PREVENT RUN-ON OR TRACKING ONTO POROUS PAVEMENTS. FREQUENCY SHALL BE ADJUSTED AS NEEDED.

3.04INSPECTION, CORRECTIVE ACTION, REMOVAL AND REPLACEMENT OF BINDER COURSE

- AND REPLACEMENT OF BINDER COURSE

 A. PRIOR TO INSTALLATION OF THE POROUS
 ASPHALT WEARING COURSE, THE BINDER
 COURSE WILL BE INSPECTED FOR DAMAGE AND
 REDUCED INFILTRATION CAPACITY.

 B. BINDER COURSE CLOGGING WILL BE
- CORRECTED BY VACUUM AND PRESSURE WASHING TO THE SATISFACTION OF THE ENGINEER.

 C. BINDER COURSE AREAS WILL BE REMOVED AND REPLACED IN AREAS WHERE STRUCTURAL
- DAMAGE OR INFILTRATION CAPACITY IS
 SUBSTANTIALLY COMPROMISED AT THE
 DISCRETION OF THE ENGINEER.

 D. THE CHOKER COURSE SHALL BE INSPECTED
 FOR VOIDS AND FINES PRIOR TO
 REPLACEMENT OF THE BINDER COURSE. IF
 VOID SPACE IS COMPROMISED, STONE WILL BE

ADDED AND/OR REPLACED FOR CORRECTIVE

ACTION.

E. THERE WILL BE NO ADDITIONAL PAYMENT FOR BINDER COURSE REPAIRS, CLEANING, REPLACEMENT, OR ASSOCIATED WORK.

3.05 POROUS ASPHALT WEARING COURSE INSTALLATION

- A. GENERAL
 I. VERIFY BINDER COURSE CONDITION AND
 PREPARATION FOLLOWING CONSTRUCTION
 PRIOR TO PAVING THE POROUS ASPHALT
- WEARING COURSE AS DESCRIBED IN SECTION 3.07.

 2. THE ENGINEER SHALL BE NOTIFIED AND INSPECT THE BINDER COURSE AT THEIR

DISCRETION PRIOR TO PAVING THE POROUS

- ASPHALT WEARING COURSE.

 3. TEMPORARY CONSTRUCTION FENCING WILL
 BE USED TO CLOSE POROUS PAVEMENT
 AREAS TO CONSTRUCTION TRAFFIC AFTER
 PAVING DURING PROJECT COMPLETION.
- B. TEST STRIP
 I. A TEST STRIP SHALL BE CONDUCTED TO
 DETERMINE OPTIMAL COMPACTION
 PROCEDURES OF THE POROUS ASPHALT AT
 A THICKNESS AS INDICATED IN THE
 DRAWINGS. THE TEST STRIP WILL BE
 CONSTRUCTED IN A PORTION OF THE SITE
 TO ESTABLISH AND ENSURE THE PROPER
 MIX DESIGN, PRODUCTION AND PLACEMENT.
 2. THE TEST STRIP SHALL BE OVERSEEN BY
- THE ENGINEER.

 3. TWO MIX SAMPLES SHALL BE COLLECTED AT THE ASPHALT PLANT BY A 3RD PARTY OC TECHNICIAN DURING PRODUCTION FROM EACH TEST STRIP FOR ASPHALT CONTENT, GRADATION, AND CANTABRO WEAR.
- 4. FIELD TESTING OF INFILTRATION CAPACITY
 SHALL BE PERFORMED ON THE TEST STRIP
 FOR INFILTRATION BY THE ENGINEER.

 5. TWO CORES SHALL BE COLLECTED FROM
- EACH TEST STRIP AND EVALUATED FOR COMPACTION, DENSITY, AND POROSITY.

 6. THESE CRITERIA ONCE ESTABLISHED WILL BE APPLIED TO ALL POROUS ASPHALT INSTALLATIONS.
- ROLLERS
 I. ROLLERS OR OSCILLATING VIBRATORY
 ROLLERS, RANGING FROM 8-12 TONS, SHALL
 BE USED FOR COMPACTION, AND 1-2 TONS
 ROLLER FOR FINISHING. THE NUMBER, MASS
 (WEIGHT), AND TYPE OF ROLLERS FURNISHED
 SHALL BE SUFFICIENT TO OBTAIN THE
 REQUIRED COMPACTION WHILE THE MIXTURE
 IS IN A WORKABLE CONDITION. GENERALLY,
 ONE BREAKDOWN ROLLER WILL BE NEEDED
 FOR EACH PAVER USED IN THE SPREADING
 OPERATION.
- OPERATION.

 2. ADDITIONAL ROLLING MAY BE EXCESSIVE, CAUSING A BREAK IN THE BOND OF ASPHALT BETWEEN AGGREGATE PARTICLES, PARTICLARLY AFTER THE MIX HAS
- COOLED.

 3. TO PREVENT ADHESION OF THE MIXTURE TO THE ROLLS, ROLLS SHALL BE KEPT MOIST WITH WATER OR WATER MIXED WITH VERY SMALL QUANTITIES OF DETERGENT OR OTHER APPROVED MATERIAL. EXCESS
- LIQUID WILL NOT BE PERMITTED.

 4. OTHER COMBINATIONS OF ROLLERS AND/OR METHODS OF COMPACTING MAY BE USED IF APPROVED IN WRITING BY THE ENGINEER, PROVIDED THE COMPACTION REQUIREMENTS ARE MET. THE SPEED OF THE ROLLER SHALL BE SLOW AND UNIFORM TO AVOID DISPLACEMENT OF THE MIXTURE, AND THE ROLLER SHOULD BE KEPT IN AS CONTINUOUS OPERATION AS PRACTICAL. ROLLING SHALL CONTINUE UNTIL ALL ROLLER MARKS AND
- CONTINUE UNTIL ALL ROLLER MARKS AND RIDGES HAVE BEEN ELIMINATED.

 5. ROLLERS WILL NOT BE STOPPED OR PARKED ON THE FRESHLY PLACED MAT. THE SPEED OF THE ROLLER SHALL BE SLOW AND UNIFORM TO AVOID DISPLACEMENT OF THE MIXTURE, AND THE ROLLER SHOULD BE KEPT IN AS CONTINUOUS OPERATION AS PRACTICAL. ROLLING SHALL CONTINUE UNTIL ALL ROLLER MARKS AND RIDGES HAVE
- BEEN ELIMINATED.

 6. ROLLERS WILL NOT BE STOPPED OR PARKED ON THE FRESHLY PLACED MAT.

 7. CONDITIONING OF EXISTING SURFACE

 1. CONTACT SURFACES SUCH AS CURBING, GUTTERS, AND MANHOLES SHALL BE PAINTED WITH A THIN, UNIFORM COAT OF TYPE RS-I EMULSIFIED ASPHALT IMMEDIATELY BEFORE THE ASPHALT MIXTURE IS PLACED AGAINST THEM.
- E. TEMPERATURE REQUIREMENTS

 I. THE TEMPERATURE OF THE ASPHALT
 MIXTURE, AT THE TIME OF DISCHARGE FROM
 THE HAUL VEHICLE AND AT THE PAVER,
 SHALL BE BETWEEN 135-163°C (275 TO
 325°F), WITHIN 6 °C (10 °F) OF THE
 COMPACTION TEMPERATURE FOR THE
 APPROVED MIX DESIGN.
- 2. THE TEMPERATURE OF THE ASPHALT MIXTURE, AT THE TIME OF DISCHARGE FROM THE HAUL VEHICLE AND AT THE PAVER, SHALL BE BETWEEN 135-163°C (275 TO 325°F), WITHIN 6 °C (10 °F) OF THE COMPACTION TEMPERATURE FOR THE APPROVED MIX DESIGN.
- 3. BREAKDOWN ROLLING SHALL OCCUR WHEN THE MIX TEMPERATURE IS BETWEEN 135-163°C (275 TO 325°F).
- 4. INTERMEDIATE ROLLING SHALL OCCUR WHEN THE MIX TEMPERATURE IS BETWEEN 93-135°C (200 TO 275°F).
- 5. FINISH ROLLING SHALL OCCUR WHEN THE MIX TEMPERATURE IS BETWEEN 66-93°C (150 TO 200°F)

- F. SPREADING AND FINISHING
 I. THE POROUS ASPHALT WEARING COURSE
 SHALL BE PLACED IN ONE APPLICATION TO
 A THICKNESS AS INDICATED ON THE
 DRAWINGS.
- 2. THE CONTRACTOR SHALL PROTECT ALL EXPOSED SURFACES THAT ARE NOT TO BE TREATED FROM DAMAGE DURING ALL PHASES OF THE PAVEMENT OPERATION.

 3. NO TRAFFIC WILL BE PERMITTED ON
- MATERIAL PLACED UNTIL THE MATERIAL HAS BEEN THOROUGHLY COMPACTED AND HAS BEEN PERMITTED TO COOL TO BELOW 36 °C (100 °F). THE ENGINEER RESERVES THE RIGHT TO REQUIRE THAT ALL WORK ADJACENT TO THE PAVEMENT, SUCH AS GUARDRAIL, CLEANUP, AND TURF ESTABLISHMENT, IS COMPLETED PRIOR TO PLACING THE WEARING COURSE WHEN THIS WORK COULD CAUSE DAMAGE TO THE PAVEMENT.
- I. PLACING MIX IN AN APPROPRIATE AMBIENT TEMPERATURE AND ON A SURFACE SUFFICIENTLY WARM TO MINIMIZE THE RISK OF EXCESSIVE COOLING BEFORE COMPLETION OF ROLLING IS OF PARAMOUNT IMPORTANCE. HOLDING THE AGGREGATE PARTICLES IN PLACE IS SOLELY THE FUNCTION OF THE FILM OF ASPHALT. THE ASPHALT CANNOT PERFORM THIS FUNCTION PROPERLY IF THE MIX IS TOO COOL WHEN ROLLED.
- 2. IT IS IMPORTANT TO KEEP THE TIME
 BETWEEN THE POROUS ASPHALT AND
 WEARING COURSE AND BINDER COURSE
 PLACEMENTS SHALL BE MINIMAL AND KEEP
 THE FIRST LAYER CLEAR FROM DUST AND
 MOISTURE, AND MINIMIZE TRAFFIC ON THE
 FIRST LAYER.
- A THIN COURSE COMPRESSES VERY LITTLE UNDER THE ROLLER AND, AS IT COOLS QUICKLY, IT MUST BE ROLLED AS SOON AS POSSIBLE.
 A HIGH DEGREE OF DENSIFICATION IS NOT

THE GOAL WITH THIS TYPE OF MIX; THE AIM

- IS FIRM SEATING AND CONTACT OF THE AGGREGATE PARTICLES.

 5. WHEN OVERTAKEN BY SUDDEN STORMS, THE ENGINEER MAY PERMIT WORK TO CONTINUE UP TO THE AMOUNT WHICH MAY BE IN TRANSIT FROM THE PLANT AT THE TIME, PROVIDED THE MIXTURE IS WITHIN
- TEMPERATURE LIMITS SPECIFIED.

 6. IMMEDIATELY AFTER THE ASPHALT MIXTURE HAS BEEN SPREAD, STRUCK OFF, AND SURFACE IRREGULARITIES ADJUSTED, IT SHALL BE THOROUGHLY AND UNIFORMLY COMPACTED BY ROLLING. THE COMPACTION OBJECTIVE IS 18% 22% IN PLACE VOID CONTENT (CORELOCK). GREAT CARE SHALL BE TAKEN TO AVOID
- OVER-COMPACTION.

 7. IF THE ENGINEER DETERMINES THAT UNSATISFACTORY COMPACTION OR SURFACE DISTORTION IS BEING OBTAINED OR DAMAGE TO HIGHWAY COMPONENTS AND/OR ADJACENT PROPERTY IS OCCURRING USING VIBRATORY COMPACTION EQUIPMENT, THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
- 8. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON DEPRESSED AREAS, EITHER A TRENCH ROLLER OR CLEATED COMPRESSION STRIPS MAY BE USED UNDER THE ROLLER TO TRANSMIT COMPRESSION TO THE DEPRESSED AREA.
- H. JOINTS
 I. UNLESS OTHERWISE SPECIFIED, THE
 LONGITUDINAL JOINTS SHALL BE ROLLED
 FIRST. NEXT, THE CONTRACTOR SHALL BEGIN
 ROLLING AT THE LOW SIDE OF THE
 PAVEMENT AND SHALL PROCEED TOWARDS
 THE CENTER OR HIGH SIDE WITH LAPPED
 ROLLINGS PARALLEL TO THE CENTERLINE.
 2. PLACEMENT OF THE SURFACE COURSE SHALL
 DE CAREELILLY BY ANDER TO AGGINE THAT
- 2. PLACEMENT OF THE SURFACE COURSE SHALL BE CAREFULLY PLANNED TO ASSURE THAT THE LONGITUDINAL JOINTS IN THE SURFACE COURSE WILL CORRESPOND WITH THE EDGES OF THE PROPOSED TRAFFIC LANES. THEY SHALL NOT BE LOCATED WITHIN THE NORMAL WHEELPATH OF VEHICULAR TRAFFIC.

 3. WHEN PAYING ADJOINING LANES, THE
- 3. WHEN PAYING ADJOINING LANES, THE ASPHALT CONCRETE SHALL BE LAID SUCH THAT IT UNIFORMLY OVERLAPS THE ADJACENT LANE 2 INCHES TO 3 INCHES. THE THICKNESS OF THE OVERLAP MATERIAL SHALL BE APPROXIMATELY 1/4 THE COMPACTED THICKNESS OF THE COURSE, SO AS TO RESULT IN A SMOOTH AND WELL COMPACTED JOINT AFTER ROLLING. THE OVERLAPPED MATERIAL SHALL BE BROOMED OR RAKED BACK ONTO THE ADJACENT HOT LANE SO THAT THE ROLLER OPERATOR CAN CROWD THE SMALL EXCESS INTO THE HOT SIDE OF THE JOINT. IF THE OVERLAP IS EXCESSIVE. THE EXCESS MATERIAL SHALL BE TRIMMED OFF SO THAT THE MATERIAL ALONG THE JOINT IS
- UNIFORM.

 4. THE COARSE PARTICLES OF AGGREGATE IN THE OVERLAP MATERIAL SHALL BE REMOVED AND WASTED IF DEEMED NECESSARY BY THE ENGINEER.

 5. TRANS/EPSE MINTS SHALL BE STAGGERED.
- 5. TRANSVERSE JOINTS SHALL BE STAGGERED
 A MINIMUM OF IO FEET FROM ADJACENT
 LANES.
 TRAFFIC
 I. ALL TRUCKS (INCLUDING CONCRETE TRUCKS)
- SITE AND INSTRUCTED AS TO SPECIAL CONCERNS FOR PAVEMENT DURABILITY.

 2. TRUCKS AND OTHER CONSTRUCTION TRAFFIC WILL NOT BE ALLOWED TO ACCESS THE SITE WHILE THE PAVEMENT IS EXCESSIVELY HOT.

 3. POROUS PAVEMENT SURFACE SHALL BE

WILL BE STOPPED PRIOR TO ENTERING THE

PAVEMENT CURE PERIOD (I-2 WEEKS).

4. A TEMPERATURE GUN SHALL BE AVAILABLE ONSITE TO ASSESS PAVEMENT SURFACE TEMPERATURES. PAVEMENT TEMPERATURES IN EXCESS OF IOOOF SHOULD BE OBSERVED CAREFULLY FOR PAVEMENT DURABILITY.

PROTECTED ON HOT DAYS DURING THE

- 5. COOLING OF PAVEMENT SURFACE BY APPLICATION OF WATER FROM A WATER TRUCK SHOULD OCCUR WHEN HEAVY VEHICULAR TRAFFIC IS EXPECTED, SUCH AS CONCRETE TRUCKS FOR CURB INSTALLATION. IN THE EVENT THIS IS INEFFECTIVE FOR COOLING AND PAVEMENT DEFORMATION IS STILL OBSERVED, 3/4" PLYWOOD SHALL BE PLACED ON TOP OF THE PAVEMENT.
- THE PAVEMENT.

 6. AFTER A 24 HOUR CURING PERIOD OF THE POROUS ASPHALT WEARING COURSE, LIMITED TRAFFIC MAY BE ROUTED OVER THE FINISHED SURFACE.
- TRAVEL OF CONSTRUCTION EQUIPMENT, AND TRAFFIC IS ALLOWED OVER THE BINDER COURSE ROAD.
 TRACKING OF DEBRIS SHALL BE MINIMIZED TO A FEASIBLE EXTENT DURING
- CONSTRUCTION THROUGH THE USE OF STONE ENTRANCES, AND ROUTINE PAVEMENT VACUUMING.

 9. UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER, CONSTRUCTION EQUIPMENT, AND TRAFFIC SHALL BE PROHIBITED FROM TRAVELING OVER THE COMPLETED POROUS ASPHALT SURFACE UNTIL THE ENTIRE
- IO. DAMAGE TO THE BINDER COURSE LAYER
 CAUSED BY CONSTRUCTION EQUIPMENT OR
 TRAFFIC SHALL BE REMEDIED BY
 COMPLETE REMOVAL AND REPLACEMENT OF
 THE DAMAGED AREA TO THE LIMITS
 DETERMINED BY THE ENGINEER.
 J. OTHER

PAVEMENT STRUCTURE IS IN PLACE.

- I. OTHER COMBINATIONS OF ROLLERS AND/OR METHODS OF COMPACTING MAY BE USED IF APPROVED IN WRITING BY THE ENGINEER, PROVIDED THE COMPACTION REQUIREMENTS ARE MET.
- A WASHOUT AREA FOR ALL CONCRETE
 TRUCKS SHALL BE DESIGNATED OUTSIDE OF
 POROUS PAVEMENT AREA ON EROSION
 CONTROL PLAN.
 NO STOCKPILING OF MATERIALS (SOIL,
- STONE, LANDSCAPING MATERIALS, ETC.)
 WILL BE ALLOWED ON POROUS PAVEMENTS.

 4. MATERIALS EXCAVATED FOR CURB
 INSTALLATION AND LANDSCAPING
 STOCKPILES SHALL BE PLACED OUTSIDE OF
- POROUS PAVEMENT AREA.

 5. WEEKLY VACUUMING OF THE POROUS
 PAVEMENT AND SURROUNDING AREAS
 (20-FEET BEYOND LIMITS OF POROUS
 PAVEMENT) SHALL BE PERFORMED BY THE
 CONTRACTOR AS NECESSARY, FREQUENCY
 SHALL BE INCREASED IF REQUIRED BY
 CONSTRUCTION ACTIVITIES.
- 3.06 DRIVEWAYS AND PARKING AREAS

 A. PAVING MATERIALS, TYPE OF PAVING, DEPTH
 OF VALUE OF PAVING, SHALL BE AS
- SHOWN ON THE DRAWINGS.

 I. THE DRIVEWAYS AND PARKING AREAS SHALL BE CUT BACK I2 INCHES FROM OUTSIDE DISTURBED OR DAMAGED AREAS AS DESCRIBED ABOVE.

 2. THE MINIMUM DEPTH OF SUBBASE SHALL BE
- AS INDICATED ON THE DRAWINGS AND NHDOT AGGREGATE SPECIFICATIONS.

 3. THE WORK SHALL INCLUDE PROPER COMPACTION OF ANY NECESSARY SUBBASE,
- BASE COURSE AND PAVING COURSES.

 B. BITUMINOUS SURFACES SHALL BE RESTORED WITH ASPHALT CONCRETE MATCHING EXISTING, BUT IN NO CASE SHALL BE LESS THAN 2 INCHES OF BINDER AND I INCH OF TOP COURSE AS SPECIFIED IN THE APPLICABLE ARTICLES OF THIS SECTION
- THE DRAWINGS, CONSTRUCT NEW DRIVEWAYS
 AND PARKING AREAS OR RESTORE EXISTING
 DRIVEWAYS AND PARKING AREAS AS
 FOLLOWS:
 I. GRAVEL SURFACES SHALL BE RESTORED
 USING SCREENED GRAVEL, MATCHING
 EXISTING, BUT IN NO CASE SHALL BE LESS

NON-BITUMINOUS SURFACES - WHERE SHOWN ON

- THAN 6 INCHES THICK. THE GRAVEL SHALL BE GRADED, SHAPED AND COMPACTED. LOOSE STONES SHALL BE REMOVED.

 2. CRUSHED STONE SURFACES SHALL BE RESTORED MATCHING EXISTING STONE, BUT IN NO CASE SHALL BE LESS THAN I INCH THICKNESS OF STONE. STONE SHALL BE
- COMPACTED WITH A ROLLER.

 3.07 SEAL AND TACK COAT
 A. APPLY SEAL COAT TO DENSE MIX ASPHALT
 AND ASPHALT CURBS ONLY AND IN
 ACCORDANCE WITH NHOOT STANDARD
- B. SEAL COAT APPLICATION TO POROUS
 PAVEMENTS IS PROHIBITED.
 C. BITUMINOUS MATERIAL FOR THE TACK COAT
 SHALL BE EMULSIFIED ASPHALT, GRADE RS-I
 CONFORMING TO NHDOT SPECIFICATION.

SPECIFICATIONS.

- 3.08 TOLERANCES
 A. SURFACE TOLERANCE THE PAVEMENT SURFACE SHALL BE CONSTRUCTED TO A I/4-INCH TOLERANCE. IF, IN THE OPINION OF THE ENGINEER, THE PAVEMENT SURFACE IS NOT BEING CONSTRUCTED OR HAS NOT BEEN CONSTRUCTED TO THIS TOLERANCE BASED UPON VISUAL OBSERVATION OR UPON RIDING QUALITY, HE MAY TEST THE SURFACE WITH A I6-FOOT STRAIGHT EDGE (FURNISHED BY THE CONTRACTOR) OR STRING LINE PLACED PARALLEL TO THE CENTERLINE OF THE PAVEMENT AND WITH A IO-FOOT STRAIGHT EDGE OR STRING LINE PLACED TRANSVERSELY TO THE CENTERLINE OF THE PAVEMENT ON ANY
- SATISFACTORILY CORRECTED OR THE PAVEMENT RELAYED AT NO ADDITIONAL COST AS ORDERED BY THE ENGINEER.

 B. THICKNESS TOLERANCE THE THICKNESS INDICATED FOR EACH OF THE VARIOUS COURSES OF BITUMINOUS PAVEMENT IS THE NOMINAL THICKNESS. THE PAVEMENT SHALL BE SO CONSTRUCTED THAT THE FINAL COMPACTED THICKNESS IS AS NEAR TO THE NOMINAL THICKNESS AS IS PRACTICAL, AND WITHIN THE

VARIATIONS EXCEEDING 1/4-INCH SHALL BE

PORTION OF THE PAVEMENT.

TOLERANCES SPECIFIED BELOW.

I. MATERIAL WHICH IS PART OF A TRUING OR LEVELING COURSE OR SHIM COURSE WILL NOT BE CONSIDERED IN PAVEMENT THICKNESS DETERMINATIONS.

2. A TOLERANCE NOT TO EXCEED 1/4-INCH FROM THE NOMINAL THICKNESS REQUIRED FOR THE COURSE SPECIFIED UNDER ONE PAY ITEM WILL BE ACCEPTABLE WHERE THE REQUIRED NOMINAL THICKNESS IS 4 INCHES OR LESS, A TOLERANCE NOT TO EXCEED 1/2-INCH FROM THE NOMINAL THICKNESS REQUIRED FOR THE COURSE OR COURSES SPECIFIED UNDER ONE PAY ITEM WILL BE ACCEPTABLE WHERE THE REQUIRED NOMINAL THICKNESS IS OVER 4 INCHES. IN ADDITION. THE SUM TOTAL THICKNESS OF ALL BITUMINOUS MIXTURE COURSES SHALL NOT VARY FROM THE TOTAL OF THE NOMINAL THICKNESS INDICATED ON THE PLANS BY MORE THAN 1/4-INCH WHERE THE TOTAL NOMINAL THICKNESS IS 4 INCHES OR LESS; OR MORE THAN 1/2-INCH WHERE THE TOTAL NOMINAL THICKNESS IS OVER 4 INCHES BUT NOT MORE THAN & INCHES: AND BY NOT MORE THAN 5/8-INCH WHERE THE TOTAL NOMINAL THICKNESS IS MORE THAN 8

INCHES. 3.09 PROTECTION

- A. ANY PAVEMENT, CONSTRUCTED OR
 RECONSTRUCTED, WHICH IS SUBSEQUENTLY
 DAMAGED DUE TO ACTIVITY OF WORK UNDER
 THIS CONTRACT, SHALL BE REMOVED AND
 REPLACED BY THE CONTRACTOR AT NO
- ADDITIONAL COST TO THE OWNER.

 B. PROTECT PAVEMENT FROM VEHICULAR TRAFFIC UNTIL COMPACTION IS COMPLETED.
- 3.10 PAVEMENT MARKING
 A. PAVEMENT MARKING SHALL BE IN
 ACCORDANCE WITH NHDOT STANDARD
- SPECIFICATION.

 B. STRIPING PAINT FOR POROUS ASPHALT SHALL BE LATEX, WATER-BASE EMULSION, READY-MIXED, AND COMPLYING WITH PAVEMENT MARKING SPECIFICATIONS PS TT-P-1952 AND IN ACCORDANCE WITH UNHSC DESIGN SPECIFICATIONS FOR POROUS ASPHALT PAVEMENT AND INFILTRATION BEDS, REV. OCTOBER 2009 OR MOST RECENT UPDATE LOCATED AT HTTP://WWW.UNH.EDU/UNHSC/SPECS-AND-FACT

PART 4: QUALITY ASSURANCE AND QUALITY CONTROL

4.01 GENERAL A PERFORM WORK

-SHEETS-O

- A. PERFORM WORK IN ACCORDANCE WITH THE NHDOT STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES, AS AMENDED TO DATE AND AS THEY APPLY TO THE FOLLOWING AND UNHSC DESIGN SPECIFICATIONS FOR POROUS ASPHALT PAVEMENT AND INFILTRATION BEDS OR MOST RECENT UPDATE LOCATED AT HTTP://WWW.UNH.EDU/UNHSC/SPECS-AND-FACT-SHEETS-O.:
- MATERIALS AND BATCH PLANT
 REQUIREMENTS.
- CONSTRUCTION PROCEDURES EXCEPT AS MODIFIED HEREIN.
 WEATHER AND SEASONAL LIMITATIONS
- EXCEPT AS MODIFIED HEREIN.

 A. PAVING WORK SHALL BE PERFORMED BY A
 QUALIFIED PAVING CONTRACTOR OR
 SUBCONTRACTOR ACCEPTABLE TO THE OWNER
- AND ENGINEER.

 B. OBTAIN ASPHALT CONCRETE MATERIALS FROM SAME SOURCE THROUGHOUT PROJECT.

4.02 QUALITY ASSURANCE/CONTROL DURING SUBBASE AND SUBGRADE PREPARATION

- 4.03 THIRD PARTY QUALITY CONTROL OF HOT MIX PLANT PRODUCTION A. THE CONTRACTOR SHALL PROVIDE AT
- A. THE CONTRACTOR SHALL PROVIDE AT CONTRACTOR'S EXPENSE AND THE ENGINEER'S APPROVAL A THIRD PARTY QUALITY CONTROL INSPECTOR TO OVERSEE AND DOCUMENT BOTH I) MIX PRODUCTION OF THE POROUS ASPHALT WEARING COURSE AND BINDER COURSE MATERIALS USED FOR THE PROJECT, AND 2) FIELD TESTING FOR IN PLACE MATERIALS, AS
- PER TABLE 5.

 B. ALL MIX TESTING RESULTS DURING PRODUCTION SHOULD BE SUBMITTED TO THE ENGINEER ON A DAILY BASIS.
- C. QUALITY ASSURANCE FIELD TESTING SHALL BE CONDUCTED FOR IN-PLACE MATERIALS FOR THE POROUS ASPHALT SYSTEM BY A THIRD PARTY AT THE CONTRACTOR'S EXPENSE IN COORDINATION WITH THE ENGINEER.

THIRD PARTY QUALITY CONTROL FIRM TO

D. THE CONTRACTOR SHALL COORDINATE WITH THE

SUBMIT RESULTS TO THE ENGINEER DAILY AND

ON AN ON-GOING BASIS. 4.04 REVIEW SUBMITTALS OF QUALITY ASSURANCE/CONTROL OF PRODUCTION

A. PROVIDE CERTIFICATION OF APPROVED JOB MIX FORMULAS FOR TYPES TO BE USED ON THIS PROJECT.
 B. THE MIXING PLANT SHALL EMPLOY A QUALITY CONTROL INSPECTOR. THE INSPECTOR WILL PERFORM QA/QC TESTING AND WILL BE CERTIFIED IN THE DISCIPLINE OF HMA PLANT TECHNICIAN BY THE RELEVANT CERTIFYING AGENCY (E.G. NETTCP IN NEW ENGLAND). THE

CONTRACTOR SHALL SAMPLE, TEST AND

- EVALUATE THE MIX IN ACCORDANCE WITH THE METHODS AND MINIMUM FREQUENCIES SUMMARIZED IN TABLE 5.

 C. IF AN ANALYZED SAMPLE IS OUTSIDE THE TESTING TOLERANCES IMMEDIATE CORRECTIVE ACTION WILL BE TAKEN. AFTER THE CORRECTIVE ACTION HAD BEEN TAKEN THE RESULTING MIX WILL BE SAMPLED AND TESTED. IF THE RE-SAMPLED MIX TEST VALUES ARE OUTSIDE THE TOLERANCES THE ENGINEER WILL BE IMMEDIATELY INFORMED. THE ENGINEER MAY DETERMINE THAT IT IS IN THE BEST INTEREST OF THE PROJECT THAT PRODUCTION IS CEASED.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL MIX PRODUCED FOR THE PROJECT.

 D. TESTING TOLERANCES DURING CONSTRUCTION. THE PAVING MIXTURE PRODUCED SHOULD NOT VARY FROM THE DESIGN CRITERIA FOR AGGREGATE GRADATION AND BINDER CONTENT BY MORE THAN +/- 5 PERCENT (%) OR APPROVAL BY ENGINEER.
- E. SHOULD THE PAVING MIXTURE PRODUCED VARY FROM THE DESIGNATED GRADING AND ASPHALT CONTENT SPECIFIED BY MORE THAN THE ABOVE TOLERANCES, THE APPROPRIATE PRODUCTION MODIFICATIONS ARE TO BE MADE UNTIL THE POROUS ASPHALT MIX IS WITHIN THESE TOLERANCES.

4.05 QUALITY ASSURANCE/CONTROL DURING PAVING A. QA/QC REQUIREMENTS DURING PAVING ARE

SUMMARIZED IN TABLE 4 AND TABLE 5
B. MONITOR QUALITY CONTROL OVER SUPPLIERS, MANUFACTURERS, PRODUCTS, SERVICES, SITE CONDITIONS, AND WORKMANSHIP, TO PRODUCE WORK OF SPECIFIED QUALITY.

PART 5: SIGNAGE FOR OPERATIONS AND MAINTENANCE

RECOMMENDED SIGNAGE SHOULD READ AS FOLLOWS:

- POROUS PAVEMENT PARKING FACILITY FOR STORMWATER MANAGEMENT POROUS ASPHALT ROADWAY-ENVIRONMENTALLY FRIENDLY STREET
- REDUCES POLLUTION AND FLOODING FROM STORMWATER
 PROTECTS AND CONSERVES WATER RESOURCES
- REDUCES EXCESS URBAN HEAT
 REDUCES WINTER SALT FOR DEICING
 ROUTINE MAINTENANCE
- SANDING, SEALCOATING, AND CRACKSEALING PROHIBITED.
 DO NOT STORE STOCKPILES ON POROUS SURFACE SUCH AS SAND, SALT, MULCH, LOAM, OR
- GRASS CLIPPINGS.

 VACUUM 3X PER YEAR (SPRING, SUMMER, FALL)
 OR AS NEEDED.
- POWER-WASH AT A 45° ANGLE FOR CHRONICALLY CLOGGED AREAS.
 ALL SWEEPINGS MUST BE DISPOSED OF IN A
- LEGAL MANNER.
 PREVENT RUN-ON OF SEDIMENT AND DEBRIS
 THROUGH EROSION CONTROL OF NEARBY AREAS.
- MINTER MAINTENANCE
 MECHANICAL REMOVAL OF SNOW AND ICE BY
- SNOW PLOW.

 APPLY DEICING TREATMENTS DURING, AND AFTER STORMS AS NECESSARY TO CONTROL COMPACT SNOW AND ICE NOT REMOVED BY PLOWING.
- LITTLE OR NO ROAD SALT MAY BE NECESSARY
 BETWEEN STORMS. UP TO 75% REDUCED USE OF
 ROAD SALT MAY BE FEASIBLE AND MINIMIZE
 POTENTIAL IMPACT TO GROUNDWATER AND
 WEST AND RESOURCES.
- WETLAND RESOURCES.

 NO DEICING MATERIALS SHALL BE STORED ON

TABLE 1: SUB-BASE MATERIAL GRADATION REQUIREMENTS

MATERIAL TYPE	CHOKER COURSE	RESERVOIR COURSE PEA GRAVEL ASTM NO. 67	FILTER COURSE (NHDOT 304.1 MODIFIED)	
SIEVE DESIGNATION (INCH/MM)	PERCENT PASSING (%), CRITERIA	PERCENT PASSING (%), CRITERIA	PERCENT PASSING (%), CRITERIA	
6/150			100	
2/50				
11/4/37.5	100			
1/25	75 – 100	100		
% /19	45 - 65	90-100		
1/4/12.5	20 - 35			
³/ ₈ /9.5	0- 25	20-55		
#4/4.75		0-10	70-100	
#8/2.36		0-5		
#200/0.075			0-6**	
VOIDS (ASTM C29)	>40%	>40%		

TABLE 2: TESTING REQUIREMENTS FOR COMPACTION AND INFILTRATION FOR SUBGRADE AND SUBBASE

CONSTRUCTION ELEMENT	LAYER THICKNESS (IN)	FIELD TESTING REQ.	N LEVEL STANDARD PROCTOR (D698)	INFILTRATION RATE (FT/DAY)	FREQUE NCY 1X PER SF
EMBANKMENTS AND FILLS					
NON-POROUS AREAS FILL ND SUBGRADE UNDER TREETS, PARKING AREAS, AND THER PAVED AREAS	12	T138	95%	N/A	
POROUS AREAS FILL AND	12	T138	95-99%	>5-30°	10,000
JBGRADE		D3385			
POROUS AREAS SUBBASE	12	T138	95-99%	>5-30°	40,000
ILTER COURSE)		D3385			
ROUGH SITE GRADING	24	T138	85%	N/A	
TRENCHES					
PIPE SIDEFILLS AND TOP 4 EET OF PIPE BACKFILL UNDER AVEMENTS"	12	T138	93%	N/A	
BACKFILL BELOW 4 FEET NDER PAVEMENT	18	T138	90%	N/A	
THE INFILTRATION RATE SHALL BE STANDARD PROCTOR COMPACTION					

SUBBASE REFERS TO CONSTRUCTED PAVEMENT MATERIALS ABOVE SUBGRADE AND BELOW PAVEMENT SURFACE

TABLE 3: POROUS ASPHALT MIX DESIGN CRITERIA.

SIEVE DESIGNATION (INCH/MM)	PERCENT PASSING (%), CRITERIA
0.75/19	100
0.50/12.5	85-100
0.375/9.5	55-75
NO.4/4.75	10-25
NO.8/2.36	5-10
NO.200/0.075 (#200)	2-4
PGAB CONTENT (AASHTO T164)	5.7-6.2%
MIXING TEMPERATURE RANGE	290°F-350°F OR AS PER PGAB SUPPLIER
PGAB GRADE	PG 76-28
STYRENE BUTADIENE STYRENE (SBS)	3% OR TBD
AIR VOID CONTENT (ASTM D6752/AASHTO T275)	18.0-22.0%
DRAINDOWN (ASTM D6390)	< 0.3 %
RETAINED TENSILE STRENGTH (TSR) (AASHTO 283)	> 80 %
CANTABRO ABRASION TEST ON UNAGED SAMPLES (ASTM D7064-04)	< 12%

(ASTM D7064-04) < 12%

*TESTING TOLERANCES SHOULD BE WITHIN THE SPECIFIED RANGE, OR FOR SINGLE CRITERIA SHOULD NOT VARY BY MORE THAN +/- 5 PERCENT (%) OR BY APPROVAL OF ENGINEER.

**CELLULOSE OR MINERAL FIBERS MAY BE USED TO REDUCE DRAINDOWN.

***IF THE TSR (RETAINED TENSILE STRENGTH) VALUES FALL BELOW 80% WHEN TESTED PER NAPA IS 131 (WITH A SINGLE

AS HYDRATED LIME (ASTM 0977) OR A FATTY AMINE, TO RAISE THE TSR VALUE ABOVE 80% *****UNDER NO CIRCUMSTANCES IS A PGAB 64-28 ACCEPTABLE IN REPLACE OF PG 76-28.

FREEZE THAW CYCLE RATHER THAN 5). THEN IN STEP 4. THE CONTRACTOR SHALL EMPLOY AN ANTISTRIP ADDITIVE, SUCH

TABLE 4: 0

ACTIVITY	SCHEDULE/ FREQUENCY	TOLERANCE
INSPECT TRUCK BEDS FOR POOLING (DRAIN DOWN)	EVERY TRUCK	N/A
TAKE SURFACE TEMP. BEHIND JOINT HEATER	EACH PULL	6°C (10°F) OF COMPACTION TEMP.
CONSULT WITH ENGINEER TO DETERMINE LOCATIONS OF BUTT JOINTS	AS NEEDED	N/A
TEST SURFACE SMOOTHNESS & POSITIVE DRAINAGE WITH 10 FT STRAIGHTEDGE	AFTER COMPACTION	4.5 MM (3/16")
CONSULT WITH ENGINEER TO MARK CORE LOCATIONS FOR QA TESTING	AFTER COMPACTION	N/A

TABLE 5: QA/QC TESTING REQUIREMENTS BY SAMPLES AT ASPHALT PLANT AND FIELD SAMPLES BY CORE

TEST	POROUS ASPHALT WEARING COURSE RANGE/SAMPLE LOCATION	ASPHALT TREATED PERMEABLE BASE RANGE/SAMPLE LOCATION	FREQUENCY MINIMUM X PER DAY, CRITERIA
BINDER CONTENT (AASHTO T164)	5.7 – 6.25%, ASPHALT PLANT	2.7%, ASPHALT PLANT	2X, PER 500 TONS
AIR VOID CONTENT (ASTM D6752/AASHTO T275)	16 – 22%, FIELD CORE	>27%, FIELD CORES	2X, PER 500 TONS
DRAINDOWN (ASTM D6390)	≤0.3%, ASPHALT PLANT	N/A	2X, PER 500 TONS
CANTABRO ABRASION TEST ON UNAGED SAMPLES (ASTM D7064-04)	≤12%, ASPHALT PLANT	N/A	2X, PER 500 TONS
INFILTRATION RATE (HOSE TEST)*	>1000 IN/HR	>3000 IN/HR	2X, PER 500 TONS

*INFILTRATION WILL BE REDUCED FOR TESTS PERFORMED ON SLOPE; HOSE TESTS WILL BE PERFORMED BY USE OF A WATER TRUCK WITH HOSE AND MEASURED FLOW, AND DETERMINED BY DIAMETER OF PUDDLE. **INFILTRATION TESTS WILL BE COLLOCATED WITH CORE SAMPLES. ***TESTING TOLERANCES SHOULD NOT VARY FROM THE DESIGN CRITERIA BY MORE THAN +/- 5 PERCENT (%) OR APPROVAL

1	JAN 16, 2019	FOR A	PPROVAL			
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CLIENT:

SOPHIE ROBINSON

ROBINWOOD CENTER

61 STRATHAM HEIGHTS ROAD

STRATHAM, NH 03885

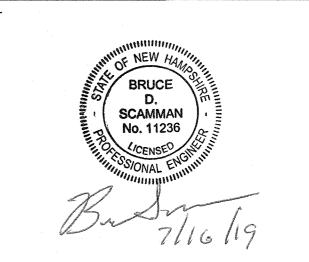
TITLE:

civil & structural consultants, land planners

118 PORTSMOUTH AVENUE, A202

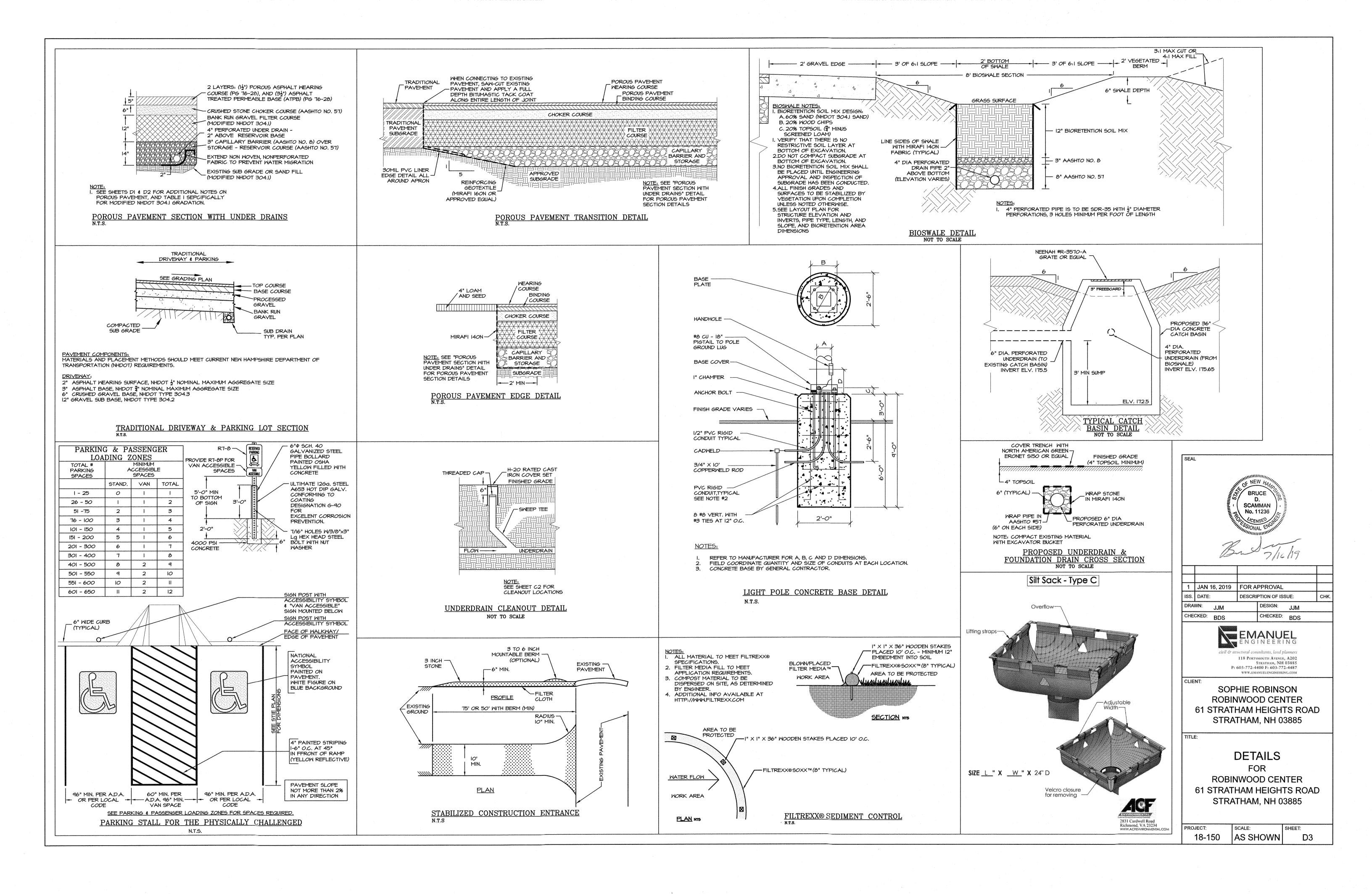
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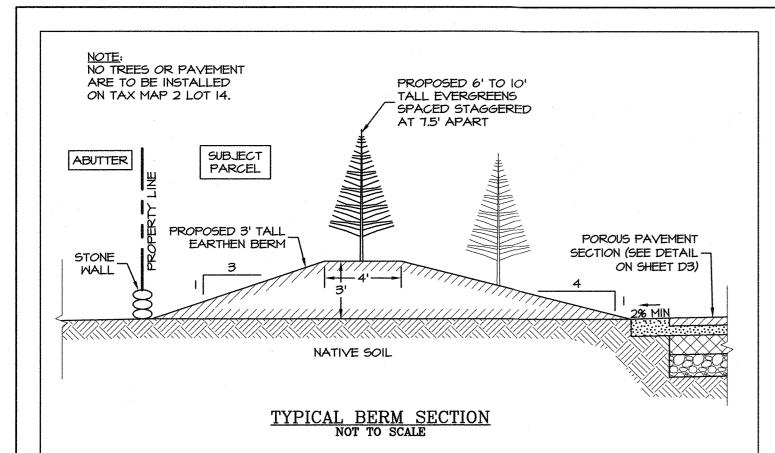
STRATHAM, NH 03885



NOTES
FOR
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

PROJECT: SCALE: SHEET: D2







WALL MOUNT FIXTURE

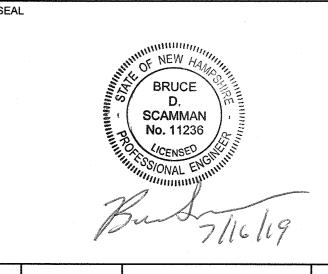
MANUFACTURER: BETA LED
MODEL: SEC EDG 4M WM 08 C UL XX (COLOR)
LAMP WATTS / TYPE: LED
INITIAL LUMENS: 6677
MOUNTING: EXT WALL MOUNT AT 12 FEET
COORDINATE COLOR WITH STEEL MANUFACTURER OR

ARCHITECT
COORDINATE WITH AVAILABLE VOLTAGE

LIGHT POLE

MANUFACTURER: HUBBELL SPAULDING LIGHT
MODEL: CLI-A-30L-I U-5K-4 TYPEIV-DB-BL
LAMP WATTS / TYPE: LED
BL-BI LEVEL CONTROL
VOLTS: 240
LUMENS: 8088
LLF: I.OO
I/WATTS: TO
T/WATTS: TO

T/WATTS: TO
MOUNTING: IT' SQUARE POLE ON 3' BASE
(20' TOTAL HEIGHT)
IDA APPROVED DARK SKY FRIENDLY FIXTURE
MOTION SENSOR
FULL CUT OFF



1 FEB 29 2019 FOR APPROVAL

ISS. DATE: DESCRIPTION OF ISSUE: CHK.

DRAWN: JJM DESIGN: JJM

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CLIENT:

SOPHIE ROBINSON
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

TITL

DETAILS
FOR
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

PROJECT: SCALE: SHEET: D4

© AMY DUTTON HOME
DRAMINGS USED
EXPRESSIVELY FOR DESIGN
ONLY FOR NOTED CLIENT. ALL
STRUCTURAL ENGINEERING
PROVIDED BY OTHER.

DATE:

1/16/19

SCALE: 1/4" = 1'-0"

SHEET:

A12



SOUTH ELEVATION | FRONT VIEW

SCALE: 1/4" = 1'-0"

NOTE: ALL EXISTING DIMENSIONS ARE TO ROUGH FRAMING / EXISTING POSTS WHICH ARE NOT CONSISTENT IN FIELD. THEREFORE, FIELD VERIFY ALL DIMENSIONS.

Dear Stratham Planning Department and Planning Board,

As an abutter at 9 Orchard Hill Road, I am writing to you to address the request for amendment to the site plan at 61 Stratham Heights Road property. Firstly, I would like to say that we would welcome any improvement to the plan that improves shielding, both of unsightly view, light and noise pollution. However, I do not see a concrete proposal here.

I would like to point out the original site plan was sorted out between abutters and the Saltonstalls over three years ago and was a result of attending multiple meetings of the planning board as well as multiple mediations between the abutters and the Saltonstalls. The trees required by the site plan have been out of compliance for the past two years. Now that they are finally being forced to address it, you are asking us to come to another meeting to discuss changing the plan with no clear proposal and no timeline.

The former residents abutting at 55 Stratham Heights Road chose to move specifically because of their dissatisfaction with the parking lot and associated zoning changes. The berm with more mature trees protecting 55 Stratham Heights Road are still in place, shielding that abutting property. That property also has two additional buildings shielding their home.

We wanted the berm to continue along the north side of the applicant's parking lot as well but only having the trees as a shield was a compromise. Perhaps if the berm was installed, all the trees shielding our home would have lived. Nonetheless, the trees should have grown several feet by now and actually be to the point where we wouldn't have to see an 80 car parking lot from our bedroom window.

Please see my key points below. I will reference the Site Plan Review Application page numbers if possible:

- 1. On pg#2. Suggesting why the trees died or that they can't be replaced, is unfounded personal conjecture. At the very least an arborist should have been consulted. Maybe the addition of a berm, or simply moving the trees a few feet away from the edge of the parking lot may have been the solution.
- 2. On pg#3. The specifics of the original site plan was created by the applicants, not forced on them by either the abutters or the planning board. The only direction from the planning board was that a berm and trees be added to shield the abutters.
- 3. On Pg#4. There was no "high quality property management plan" mentioned until now. In fact, when we very politely asked when they were going to replant the trees (last summer), they told us they didn't have to and that they planted some nice sunflowers there instead. They also suggested that if we were to pay to replant the trees, they would consider it. The only reason they are addressing this now is because the town is forcing them to.
- 4. Also on Pg#4. How does this "very special space" benefit our town far into the future and what does that have to do with shielding a parking lot? This parking lot only benefits the applicants.

- 5. On Pg#6. Corn is not an adequate replacement for maturing pine trees, which was the long-term goal. Corn will never grow tall enough to shield our second-floor bedroom windows from the parking lot, however, trees will. Corn provides no acoustic barrier and dies in the fall. Usage of the parking lot is not seasonally restricted.
- 6. Also on Pg#6. It is untrue to categorize this as "an ongoing conversation between my family, our abutters, and the Town of Stratham". We are the only abutter that is no longer shielded and any attempt with a conversation has been rebuffed as mentioned above.
- 7. In general, the corn or the sunflowers and the "40' of existing trees" that supposedly shield our abutting property are nearly <u>all deciduous</u> and are scant protection in September and October when the majority of their events take place. The only non-deciduous trees shielding us are evergreens that <u>we</u> personally added to our side of the property at significant expense. Also, the 40' of existing trees and brush can simply be removed at the whim of the applicants. A fact that was made painfully plain to us when prior to their first big event last year, they cut down a significant portion of the shielding brush so that they could put up puptents for their guests. There is nothing in the site plan keeping them from developing and changing the landscape along our property line at will. Is the planning board willing to restrict the applicants from removing any more trees or brush from along the property line?
- 8. The un-numbered pages providing photographs of sight lines from a distance in the showing the leaf-heavy foliage of spring/summer give a pointedly one-sided seasonal snapshot of the situation. Additionally, the sight lines (shown as and blue and green arrows) are separate from the parking lot and have no bearing on the discussion.
- 9. The map showing the trees on the north side of the parking lot is from Google and is not from October of 2021, but from October of 2020, you can check it with Google Earth. Although the applicants may have grabbed the screen shot in October of 2021, the arborvitaes were removed prior to the 2021 season, making this current summer (2022) the second year the parking lot is not following the site plan.
- 10. The visual showing noise from the barn doors (regardless of being acoustically inaccurate) have no bearing on the parking lot site plan.

In summation, this has been an ongoing issue that needs action. The trees have been absent since 2020. No substantial effort has been made to address it, in fact just the opposite. The applicant's request has no concrete plan and no timeline with which to be held. The amount of time and effort that the applicants are requiring of abutters and board members to attend additional board meetings, review additional proposals and mediate them is a frustrating waste of everyone's time. If they want to implement a "high quality property management plan" then let them do so after they have made their parking lot compliant.

My final question is this. The site plan approved by the Planning Board in 2019 made the use of the parking lot contingent on the installation of the trees and a berm. Why are they being allowed to use this parking lot for two years out of compliance? From our point of view, making sure that there are trees to shield their neighbors from the nuisance of the parking lot is the absolute minimum that they could have done, and they haven't.

Thank you to the Stratham Planning Department and Planning Board for your time and attention. We appreciate your ongoing patience, time and effort to attempt to resolve this issue.

Regards, Joshua & Kimberly Cooper 9 Orchard Hill Road Stratham, NH 03885



TOWN OF STRATHAM

Incorporated 1716 10 Bunker Hill Avenue Stratham, NH 03885 Town Clerk/Tax Collector 603-772-4741 Selectmen's Office/Administration/Assessing 603-772-7391 Code Enforcement/Building Inspections/Planning 603-772-7391 Fax (All Offices) 603-775-0517

Planning Board NOTICE OF DECISION

Petition of:

Bruce Scamman, Emanuel Engineering, Inc.

Project Name:

Site Plan Application, to facilitate installation of an off-street parking area, widening of a driveway, and associated infrastructure for the Robinwood Center, at 61 & 62 Stratham Heights Road, Stratham, NH; Stratham Tax Map 2 & 5 Lots 81

&14; Exeter Tax Map 67 Lots 3 and 3-1.

Premises Affected: 61 & 62 Stratham Heights Road, Stratham, NH; Stratham Tax Map 2 & 5 Lots 81

&14; Exeter Tax Map 67 Lots 3 and 3-1

So as to permit:

The installation of an off-street parking area, widening of a driveway, and associated infrastructure for the Robinwood Center, at 61 & 62 Stratham Heights

Road, Stratham, NH.

The Stratham Planning Board, at its meeting of March 06, 2019 and after a public hearing, completed its consideration of the application for the Site Plan Review application to installation of an off-street parking area, widening of a driveway, and associated infrastructure for the Robinwood Center, at 61 & 62 Stratham Heights Road, Stratham, NH, Tax Map 2 & 5 Lots 81 &14; Exeter Tax Map 67 Lots 3 and 3-1.

The Board based its decision on plans, supporting oral and written information, and records provided by the Applicant, professional staff, consultants for both the Applicant and the Board, and abutters, as reflected in the minutes on file at the Stratham Town Hall. This information shall be incorporated into the decision by reference.

As a result of such consideration, at its meeting of March 06, 2019, the Planning Board found that the application for amendment was complete and in Substantial Compliance with the Stratham Zoning Ordinance and Site Plan Review Regulations. The Planning Board voted unanimously in favor to Approve the Site Plan Review with Conditions based on the information and stated conditions attached and incorporated hereto.

During the review process, the Applicant and its professional consultants submitted various revisions to the plans along with various supplemental memoranda and correspondence in response to requests by the Planning Board and the Planning Department that reviewed the project. All of these plans, reports and correspondence, and meeting minutes are contained in the Planning Department's files and are hereby incorporated by reference into the public record for this public hearing.

On March 06, 2019, Mr. Canada made a motion approve this application, as received on March 6, 2019, with the understanding that the operational notes as presented on Sheet C-2, specifically #1, #2, #3, #5, #6, and #8 be included in an advisory opinion memorandum to the Zoning Board of Adjustment, reviewed and approved by the planning board at the March 20, 2019 hearing, and operational notes #4 and #7 be relocated to the general operating notes in the upper right hand corner, and the February 6, 2019 staff review conditions of approval as the following:

Conditions Precedent:

- 1. Remove the cross walk and coordinate with DPW at a later date.
- 2. Applicant to coordinate with Town Assessing Department relating to current use stipulations and modifications on the property prior to recordation of the site plan.
- 3. Applicant to obtain NH DES septic approval prior to issuance of building permits.
- 4. Clerical changes of removing the snow storage from south side of Stratham Heights Road.

Mr. Canada amended the motion to include the Planning Board Chair is authorized to sign the advisory opinion memorandum instead it coming back to the board. Mr. House seconded the motion. Motion carried unanimously.

Planning Board Chair

Data



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Select Board/Administration/Assessing 603-772-7391 Code Enforcement/Building Inspections/Planning 603-772-7391 Fax (All Offices) 603-775-0517

TO: Planning Board Members

FROM: Mark Connors, Town Planner

FOR: July 13, 2022

RE: Potential Land Donation to Town – 18 Jana Lane

Former Stratham residents Richard and Marilyn Young have offered to donate a small piece of land they own at 18 Jana Lane to the Town of Stratham (see their letter to the Select Board included in the Planning Board packets). The parcel is approximately 0.84 acres and includes 150-feet of frontage along Jana Lane. The property is interesting because it directly abuts the Salt River Association Conservation Land, which is protected by a conservation easement and allows for passive recreation use by the public (including hiking, cross country skiing, picnicking, and even boating access to the Squamscott River). Additionally, the Salt River land abuts several protected parcels along the Squamscott River owned by the NH Fish and Game Department, the Town of Stratham, and land protected by a Conservation Easement held by the NH Society for the Protection of NH Forests surrounding the Turnberry development (which although protected does not provide for public access).

It may be possible to construct a trailhead and small parking area at the 18 Jana Lane property to facilitate public access to these properties, but there are some challenges. There is a culvert that carries a brook across the property and the Town believes a good deal of the land may be wetland. It does not appear buildable for a traditional single-family house lot, but again there is a reasonable possibility that it could support a trailhead and some limited parking adjacent to the right-of-way. If construction of a trailhead would require a disturbance to or crossing of a wetland, the work would require a Dredge and Fill Permit from the NH Department of Environmental Services. Depending on the impact, some mitigation would likely be required.

The Youngs have indicated that there is some upland land on the eastern side of the lot. This cannot be known for sure until the land is surveyed and flagged by a certified wetland scientist.

The Select Board has requested the input of the Conservation Commission and Planning Board/Planning Dept. before responding to the Youngs. The Youngs have requested a response to their offer by August 1. The Youngs have provided the Town permission to walk the property and some members of the Conservation Commission have requested interest in participating in a site walk of the property. If members of the Planning Board are interested, staff can arrange for one in advance of the Commission's July 27 meeting.



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TO: Planning Board Members

FROM: Mark Connors, Town Planner

FOR: July 13, 2022

RE: Pubic Hearing – Adopt Route 33 HAC Application Fees

The newly formed Route 33 Heritage Advisory Committee held their first meeting on June 16, 2022 and approved its own Rules of Procedure and Application form. The Advisory Committee discussed and recommended that the Planning Board adopt a fee structure as described below. At its June 22, 2022 meeting, the Planning Board discussed potentially increasing the application fees to better reflect staff and administrative costs. Potential fee amounts discussed at the previous meeting are shown below. If the Board is supportive of increasing the fees as previously discussed, staff has offered the following amounts shown in bold and in parentheses. Staff has kept the base fee for residential/agricultural applications the same at \$50.00 since many of these applications would not previously have required Planning Board applications and will not result in new housing units.

PROPOSED APPLICATION FEES:

Base Fee / Existing Residential/Agricultural Application: \$50.00 *No commercial uses or additional housing units proposed.*

New Residential Application: \$100.00* (\$150.00) + \$75.00* (\$100.00) per new

housing unit

Where additional housing unit(s) is/are proposed.

Minor Commercial/Mixed Use Application: \$100.00* (\$150.00)

Adaptive re-use projects, architectural and/or exterior modifications or additions/new construction in which no more than 1,500 square-feet of additional interior space is proposed.

Major Commercial/Mixed Use Application: \$200.00* (\$150.00 + \$100 per 1,000 of square-feet)

In which more than 1,500 square-feet of additional interior space is proposed.

*-Site Plan and/or Conditional Use Permit application fees may also apply.

Since the Planning Board is the regulatory body for the Heritage District, the Planning Board must

approve the fee structure as part of a public hearing. At its June 22 meeting, the Board set the public hearing for July 13, 2022. The Planning Board has the option to see the fees however it sees fit. Staff will be prepared to discuss how other communities model their fees.

DRAFT MOTION:

I move to amend Section 4.2.7 of the Stratham Site Plan Regulations to set the fees for the Route 33 Heritage District as follows:

June 30, 2022

Stratham Select Board C/O David Moore, Town Administrator 10 Bunker Hill Avenue Stratham, NH 03820

RE: Potential land donation of 18 Jana Drive

Dear Members of the Stratham Select Board,

We own property at 18 Jana Lane in Stratham (Tax Map 24, Lot 31). The lot is approximately 0.84 acres and includes approximately 150 feet of frontage along Jana Lane. We previously lived at the abutting lot at 20 Jana Lane but sold the property and relocated out of the area. If the Town is interested, we would like to donate the property to the Town. The lot abuts the Salt River Association Conservation Area and we believe it would be beneficial for conservation and/or recreational purposes. Additionally, there may be some benefit to the Fire Department to maintain equipment on the property to serve the Jana Lane/Joyce Lane/Jason Drive/Linda Lane neighborhoods.

Although we are willing to transfer the land to the Town at no cost, the transfer of the property would be contingent on the Town being responsible for costs associated with the property transfer, including associated taxes, legal or other professional fees, document preparation (including preparation of the deed) and other costs associated with the transfer and/or the Town's due diligence of this offer. However, if we choose to be represented by our own attorney, we recognize we would be responsible for those costs and we will remain responsible for the paying property taxes on the parcel up to the date of the property transfer.

We recognize that the Board will likely want to conduct its due diligence and solicit the input of other Boards and Commissions, however we are anxious to move forward and would appreciate a response if the Town desires to accept the land donation no later than August 1, 2022. We understand that the transfer may not happen until a later date to allow for sufficient time to accommodate the need for public hearings and the preparation of the necessary transfer paperwork.

Respectfully,

Richard and Marilyn Young

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