



**Stratham Planning Board
AGENDA
July 13, 2022
Stratham Municipal Center
Hutton Room
Time: 7:00 PM**

The public may also access this meeting at the date and time above using this conference call information. Please dial **1-800-764-1559** and input **4438** when prompted for a user pin/code. Please follow the Chair's instructions delivered at the meeting in order to register comments during the public meeting.

If at any time during the meeting you have difficulty hearing the proceedings, please e-mail mconnors@strathamnh.gov.

1. Call to Order/Roll Call

2. Approval of Minutes:

- a. June 16, 2022 Minutes
- b. April 20, 2022 Minutes

3. Public Hearing:

- a. Tulip Tree, LLC (Owner), Kyle & Sophie Saltonstall (Applicants) - Request for approval of a site plan amendment to allow for modifications to the landscape plan associated with an event venue, non-profit lodge, and private school use at 61 Stratham Heights Road (Tax Map 5, Lot 81) approved by the Planning Board on March 6, 2019, Zoned Residential Agricultural.
- b. To amend Section 4.2.7 of the Site Plan Regulations for the purposes of adding application fees for applications submitted under the Route 33 Neighborhood Heritage District.

4. Public Meeting:

- a. Potential land donation of 18 Jana Lane to Town of Stratham
- b. Formation of the Pedestrian-Bicycle Sub-Committee of the Planning Board
- c. Discussion of Transportation Planning Priorities
- d. Master Plan Implementation
 - Walking, Biking, and Automobiles
- e. Miscellaneous Community Planning Issues

5. Adjournment

Full text of the agenda and related information can be found on file with the Stratham Planning Department and posted on the Town website at <https://www.strathamnh.gov/planning-board>. All interested persons may be heard. Persons needing special accommodations and/or those interested in viewing the application materials should contact the Stratham Planning Department at (603) 772-7391 ext. 180.



Stratham Planning Board Meeting Minutes
June 22, 2022
Stratham Municipal Center
Time: 7:00 pm

Member Present: Thomas House, Chair
Mike Houghton, Selectmen's Representative
Chris Zaremba, Regular Member
Pamela Hollasch, Regular Member (by telephone)
John Kunowski, Alternate Member

Members Absent: David Canada, Vice Chair

Staff Present: Mark Connors, Town Planner

1. Call to Order/Roll Call

Mr. House called the meeting to order and took roll call. Mr. House appointed Mr. Kunowski as a voting member for the meeting.

2. Approval of Minutes

a. June 1, 2022

The approval of minutes from June 1, 2022. Mr. House corrected the spelling of Mr. Zaremba's name in the draft minutes. Mr. Houghton made a motion to approve the draft minutes as amended. Mr. Zaremba seconded the motion. All voted in favor.

b. March 16, 2022

Mr. Kunowski asked if the approval of minutes from March 16, 2022 is outstanding. The minutes are not in the package and will be presented at the July 13, 2022 meeting.

3. Public Meeting:

a. Route 33 Heritage District and potential scheduling of public hearing for Route 33 Heritage District application fees.

Mr. House asked Mr. Connors to provide an update. The Route 33 committee held their first meeting on June 16, 2022. The committee approved an application and rules and procedures as to how the committee will be run. Their meetings will be held the first Wednesday of each month. The timing will work well with the Planning Board meeting schedule. The committee will

41 prepare their comments within one week to be included in the packet for the following Planning
42 Board's meeting. They discussed application fees. The Planning Board approves all Planning
43 Board application fees, so the committee can only recommend the fee amounts. Planning Boards
44 have to hold a public hearing to set the fees. Mr. Connors presented the recommended fees. Any
45 application relating to a residential or agricultural property, that does not result in new housing
46 units, would be a \$50 fee. This is also the base fee so anything that is non-applicable like
47 demolition without new construction or an application for a commercial sign that does not meet
48 the requirements would also fall under this amount. A residential application for new housing
49 units would be \$100 plus \$75 per new housing units. Adding an ADU to a house would be \$175.
50 A commercial application applying to a small area less than 1,500 square feet would be \$100. A
51 commercial or mixed use application that is larger than 1,500 square-feet would be \$200. These
52 fees would be in addition to the Planning Board fees. Mr. Zaremba asked if these fees are lower
53 than fees in other parts of town to encourage development in the district, how do they compare to
54 other parts of town? Mr. Connors replied that generally speaking, the fees are low, they are
55 much lower than building permit fees. For example, a building permit application was just
56 processed for the new medical office building and the building permit fee was \$13,000. The
57 committee is trying to keep the fees nominal.

58
59 Mr. Zaremba asked if the intent is to encourage development by keeping the fees low. Mr.
60 Connors replied yes. Mr. Kunowski stated that we still need to consider the administrative
61 process of reviewing the applications, but agreed that the fees should be as low as possible. Mr.
62 House stated that the goal is to cover the work of the planning staff. Mr. House stated that the
63 second fee is standard. Mr. Houghton asked if any of this is pertinent to new construction, that
64 this is all additions or modifications? Mr. Connors replied that it can be either. Mr. ? stated that
65 one of the fees is for a new residential application. Mr. ? stated that the commercial application
66 can include an addition. Mr. Connors said that if it a big development, like a 5,000 square foot
67 development, the site plan fee is \$100 per 1,000 square feet. Mr. Houghton stated that if you put
68 a value on a few hours worth of someone's time, it's not \$50, so he would advocate ensuring the
69 fees cover costs. Mr. House asked if Mr. Connors is seeking approval tonight because two
70 members are not present. Mr. Connors replied that the Board could schedule a public hearing
71 and debate the fees at the hearing. The Board does not need to make a final decision on the fees
72 tonight. We would advertise the intent to set fees at the hearing, but not publish the proposed fee
73 schedule.

74
75 Mr. Kunowski asked if the same fee categories apply to development in other parts of town or
76 are these categories applicable to the Route 33 Heritage District? Mr. Connors replied that
77 applications in the Heritage District would need to pay these application fees which are only
78 applicable to the Heritage District. But the fees have been kept low so as not to discourage
79 applications. Mr. Kunowski asked if something was happening in another part of town, an
80 application fee would not apply? Are there across the board application fees? Ms. Hollasch
81 joined the meeting via telephone. Mr. Connors replied that there are across the board fees, for
82 example a site plan for a new store would be \$250 for the base fee for the Planning Board plus
83 \$100 per 1,000 square feet. That applies to any site plan of this nature regardless where it is
84 located in town. Mr. Houghton is interested in seeing the actual costs for the Town. Mr. House
85 asked Ms. Hollasch if she reviewed the proposed Heritage District application fees. Ms. Hollasch

replied only cursorily. Mr. House asked if Ms. Hollasch has any questions on the fees. Ms. Hollasch replied no.

Mr. House asked Mr. Connors to go over the application form. Mr. Connors replied that it is similar to the site plan application. The owner must sign the application. If the applicant is a different person from the owner, they must also sign the application. We ask for permission to visit the property. Mr. Zaremba said the application looks straight forward. Mr. Zaremba asked once the application comes in, it is a two-step process that first goes to the Route 33 Heritage Committee and then come to the Planning Board? Mr. Connors replied yes. The Committee would give the Planning Board their meeting minutes with comments and recommendations, but the Planning Board ultimately makes the decision. Mr. House stated that there is a little bit of vetting before the project is submitted to the Planning Board. Mr. House stated that one of the goals is to help save time at the Planning Board. Ms. Hollasch has no questions on the application. Mr. House named the three members of the Heritage Committee: Alex Dardinski is the chair, Jeff Hyland is the co-chair, and Mr. House as the Planning Board representative. Mr. Connors said that Mr. Dardinski would like to send a letter to all property owners notifying them of the application process.

Mr. Houghton made a motion to set the first public meeting date to set site plan application fees for the Heritage District as July 13, 2022. Mr. Kunowski seconded the motion. All voted in favor and the motion was approved.

b. Stratham Open Space Plan

Mr. Connors discussed the presentation from the introductory meeting last week. He said he would briefly run through the same Powerpoint that was presented at that meeting.

The Capital Improvement Program budgets about \$45,000 for Open Space and Connectivity Plan in addition to the Town's other funding towards land conservation. The Town has put between \$15,000 and \$35,000 toward land conservation from 2009 through 2019. The Town's commitment to Open Space conservation intensified in 2002 when the Town passed a \$5 million bond for open space preservation. A lot of land preserved in Stratham came out of that bond commitment. There is also some modest funding in the CIP for pedestrian improvements. Mr. Connors went through the recommendations from the 2019 Master Plan related to Open Space Planning. The Master Plan recommended Management Plans for individual large open space parcels. The Plan recommended a specific long range plan for Stratham Hill Park. The Town has started that effort thanks to the work of the Trail Management Advisory Committee (TMAC). The Plan recommends preserving the remaining historic buildings in Stratham and continuing to use Conservation Easements where appropriate. Mr. Zaremba asked if using Conservation Easements changes the tax base of the land? Mr. Connors replied yes because development of the property is limited. Typically, the Town purchases the development rights which reduces the value of the property. Mr. Connors showed how population in Stratham evolved. There was little impact from the Industrial Revolution. The population doubled in the 1980s. Approximately 1,700 acres of land are protected under Conservation or Agricultural Easements in Stratham. The NH Fish and Game owns 115 acres and the Town of Stratham owns 486 acres, excluding properties like the Town Hall. About 24% of land in Stratham is protected. There are also

protected open space subdivisions with smaller lots and open space set aside. However, not all of the open space parcels are formally protected via an easement or some other permanent tool. Mr. ? asked if the open space at Treat Farm is protected. Discussion amongst the members is that they believe there is a protected easement. Mr. Connors said yes, he stated that it was more in the 1970s and 1980s that land protection was not always formalized when these subdivisions were approved. Stratham has done a better job with protecting agricultural land compared to other towns in New Hampshire. Mr. Connors showed a map of the different protected lands. There are a number of utility easements as well on the open space land and at the meeting last week it was discussed if the Town could acquire some rights to that land for hiking and pedestrian connections. Steve Whitman attended the meeting last week to present different types of open space plans. The presentation was recorded and Mr. Connors is working on getting the video online. It would be good for Stratham to get an inventory of open space and make it available to the public that details what can be accessed and what cannot. Complete an analysis of our baseline and where we want to go and complete public outreach and planning for what comes next. Mr. House requested a paper copy in the library for people without computers. Mr. Connors reiterated that creating a map with symbols of what the public can and cannot do on lands is important.

Mr. Connors presented the Open Space Plan for the City of Portsmouth. There is a Greenway from downtown to the protected land on the outskirts of town. Mr. Connors presented the Open Space Plan for Burlington, Vermont. There was a lot of outreach to people being better stewards of the land. Moose Mountain, New Hampshire got a grant for a Greenways plan with neighboring towns.

There was a question as to what Board and Committee members want the plan to focus on? Responses included protecting agricultural land, prioritizing environmentally sensitive land, clarifying the Town's and landowners responsibilities and Conservation Commission's roles, and clarifying where residents can access public land. Ms. Hollasch suggested inventorying the open space based on how the land benefits us, for example, flood control, public recreation, education, wildlife habitat etc. Mr. House suggested including maintenance of the open space properties as part of the plan. Discussion continued on suggestions for mapping Stratham's conservation areas. Mr. Connors would like the Town to work with the property owners who have easements that don't require public access but previously allowed access to their land. Ideally the Town could work with the landowners to see what can Stratham to promote better stewardship and respect of the property so public access could be regained? Mr. House suggested also talking to landowners who are considering adding trails. Ms. Hollasch noted the complication with easements held by an entity who is different from the entity managing the land therefore there are two entities that might have an opinion on how the land is used and maintained. It was suggested a new temporary committee might be needed to look at some of the items discussed. Mr. Connors stated the RFP would be released soon with Planning Board input, then to the Select Board, and then public outreach in the Fall.

c. Discussion of potential source water protection and stormwater (MS4) mitigation measures.

Mr. Connors stated there is nothing new from the last meeting.

176
177 **d. Master Plan Implementation**
178

179 Mr. Connors stated this was generally covered in the PowerPoint presentation earlier in the
180 meeting relative to the Open Space Plan but I have printed out the recommendations relating to
181 conservation right from the Master Plan if the Board has any questions. There were no questions
182 or additional discussion.
183

184 **Miscellaneous Community Planning Issues**
185

186 Mr. Connors said the Regional Planning Commission reached out to Stratham as they update the
187 long-range transportation plan. There are two plans to keep in mind, a Ten Year Plan where all
188 projects slated for the next ten years are budgeted for. The Portsmouth Avenue/Route 33 safety
189 improvements in Stratham and Greenland the and Bunker Hill Avenue/Portsmouth Avenue
190 signalization project is in the plan. Those projects are budgeted for and moving forward. The
191 second is the long range transportation plan which is like a master plan. There are five projects in
192 that plan -- the Portsmouth Ave pedestrian/bicycle improvements, reconfiguration of the
193 Stratham Traffic Circle, Squamscott Road bicycle lanes, Frying Pan Lane/Portsmouth Avenue
194 intersection improvements, and Marin Way intersection improvements. The RPC would like the
195 Town's input as to what one of these projects represents a highest priority. This is something to
196 keep on your radars, I will prepare more information for you for the next meeting.

197 **e. Adjournment:**
198

199 Mr. Houghton made a motion to adjourn at 8:30 pm. Mr. Zaremba seconded the motion. All voted in
200 favor and the motion was approved.
201



TOWN OF STRATHAM

Incorporated 1716

10 Bunker Hill Avenue · Stratham, NH 03885

Town Clerk/Tax Collector 603-772-4741

Select Board/ Administration/ Assessing 603-772-7391

Code Enforcement/Building Inspections/Planning 603-772-7391

Fax (All Offices) 603-775-0517

TO: Planning Board Members
FROM: Mark Connors, Town Planner
FOR: July 13, 2022
RE: **Request to Postpone – 61 Stratham Heights Road**

The applicants for the 61 Stratham Heights Road Planning Board application have requested that the application be postponed to the August 17, 2022 meeting as they are expecting a child birth.

The Planning Board would need to take a vote to postpone the application to a date certain. Staff takes no objection to the request and has notified abutters who have contacted the Planning Department of the request to postpone.

The original staff report for the application, application materials, abutter comments, and communications from the Building Inspector/Code Enforcement for the Planning Board's reference.

On July 6, 2022 at 22:43, kyle@saltonstallfarm.com wrote:
Mr. Connors,

As discussed in person yesterday, my wife is very, very pregnant. Our soon to be born son is already weighing in at 9 1/2 lbs according to a recent ultrasound. Sophie is scheduled for a C-section at 9am on the 22nd, but the doctors have informed us that this child could come at any moment. Sophie would prefer that this kid come right now.

Given the unknowable near future, we ask that our upcoming scheduled appearance before the Planning board be postponed until the August 17 meeting. We'd like to promise to make the meeting next week, but we are not in condition to do so at this time.

Very Respectfully,

K Kyle Saltonstall
Co-Owner, Saltonstall Farm
US Army Special Operations Veteran
cell: [\(603\)312-4806](tel:6033124806)
61 Stratham Heights Road
Stratham, NH 03885

.



TOWN OF STRATHAM

Incorporated 1716

10 Bunker Hill Avenue · Stratham, NH 03885

Town Clerk/Tax Collector 603-772-4741

Select Board/Administration/Assessing 603-772-7391

Code Enforcement/Building Inspections/Planning 603-772-7391

Fax (All Offices) 603-775-0517

TO: Planning Board Members
FROM: Mark Connors, Town Planner
FOR: July 13, 2022
RE: **Site Plan Amendment application, 61 Stratham Heights Road**

Background Information:

On March 6, 2019, the Planning Board approved a site plan to allow the construction of site improvements to support an event venue at 61 Stratham Heights Road. On March 26, 2019, the Zoning Board of Adjustment approved a special exception to operate the event venue then classified as a ‘non-profit lodge’ and ‘private school.’ The use operates out of a barn on the property and the improvements included a driveway, parking lots, and landscaping, among other features.

The site plan that the Planning Board approved included a row of 35 arborvitae trees adjacent to one of the parking areas. Although at one point the landscaping was installed, it appears many of the trees died and no longer exist.

On February 25, 2022 and May 2, 2022, the Code Enforcement Officer mailed the property owners letters noting that the site was not operating in conformance with the approved site plan due to the absence of this landscaping. The Code Enforcement Officer’s letter of May 2, 2022 provided the property owners a deadline of June 15, 2022 to install the landscaping or to submit an application to the Planning Board requesting an amendment to the site plan. The applicants have chosen the latter option and an application was timely submitted.

On July 7, 2022, the applicants submitted a request that the application be postponed to August, noting that they are expecting a baby this month. Since the application was already publicly noticed, the Board will need to vote to postpone the application to a date certain and that will serve as the public notice.

Application Information:

The applicants are requesting that the landscaping in question be replaced, as a short-term measure, with eight rows of sweet corn that they have already planted. The Saltonstalls have not provided a plan or photographs of the corn so it is difficult to visualize what they are proposing, however staff will attempt to visit the site to take photos in advance of the Planning Board meeting. The applicants have indicated that they are working with engineers and landscape architects to develop a more comprehensive site management and landscape plan “that will far exceed the intent of the

original site plan and Stratham's site plan regulations,," however they have not provided a timetable for when the plan will be completed or when it will be submitted to the Town for review.

Further, the applicants note the cost of installing landscaping and the fear that new landscaping will not survive. The applicants have also included a letter from a landscape architect, who requests "some time to consider the appropriateness of the proposed buffer in relationship to the other needs of the farm, the opportunity to entertain alternatives, and (at a minimum) the ability to delay any evergreen planting(s) till the fall to increase the chances of success."

In the application, Mr. Saltonstall notes that the original intent behind the landscaping was to "shield the Cooper residence [abutters to the use] from nuisance noise and light emitted from usage of our eco-parking area."

The Coopers have indicated that noise emitted from events and other disruptions associated with the use is a continuing nuisance. They have submitted a letter opposing the applicants' request which is included in the Planning Board packets.

Staff Recommendation:

Staff does not believe corn is a reasonable alternative to the more comprehensive landscaping that was included in the approved site plan. It is important to note that the Town did not develop the landscaping plan; the applicants developed the plan likely with some input from abutting property owners. The Town is merely enforcing the approved site plan the applicants developed. The Town would welcome the opportunity to review a revised landscape plan, but the applicants have not provided one, only the promise to submit one at a later unspecified date. It could be years before one is submitted. It has been nearly five months since the Town first formally notified the applicants of this compliance issue. Staff believes that should have provided sufficient time for the applicants to develop and provide the Town a revised plan.

New Hampshire provides a good climate for a wide diversity of landscaping provided adequate site preparation is made, irrigation is installed, and that the plantings are maintained. The applicants have not provided any technical exhibits to indicate why the land is not appropriate for landscaping.

Because the applicants have not provided a viable revised plan, staff would recommend that the Board deny the request for an amendment to the plan and uphold the original site plan but provide the applicants a 60-day window in which to make the improvements. However, staff would recommend that the Board provide the applicants some additional flexibility and allow for some minor revisions to the landscaping plan at the discretion of the Town Planner if a certified professional develops an alternative plan that meets the intent of the original landscape plan (see language of Condition #1).

DRAFT MOTION:

I move that the Planning Board deny the request for a site plan amendment because the request is not in conformance with Section 5.2 of the Stratham Site Plan Regulations. Further, the Planning Board directs the Code Enforcement Officer to enforce the approved 2019 Site Plan for 61 Stratham Heights Road, but to withhold enforcement action for a period of 60 days to allow the applicants the opportunity to bring the property in compliance. This decision is subject to the following condition:

1.) The Town Planner may approve minor revisions to the approved landscape plan, at his discretion, if a certified professional provides written justification for the revisions and if the revised plan is consistent with the purpose and intent of the approved landscape plan.



TOWN OF STRATHAM

INCORPORATED 1716

10 BUNKER HILL AVENUE • STRATHAM NH 03885

VOICE (603) 772-7391 • FAX (603) 775-0517 • www.strathamnh.gov

February 25, 2022

Tulip Tree LLC
Attn: Sophie and Kyle Saltonstall
61 Stratham Heights Road
Stratham, NH 03885

RE: 61 Stratham Heights Road, Map 5, Lot 81
Planning Board Notice of Decision dated 3/22/2019

Dear Mr. and Ms. Saltonstall,

The purpose of this letter is to follow up on the Planning Board's site plan approval decision referenced above (and attached). The board's decision was based on a site plan, recorded at the R.C.R.D., as Plan D-41746 (also attached), which illustrates thirty-five 6' to 10' tall evergreens planted on a berm along the westerly and northerly sides of a 37-space parking area. Although this work was completed, some of the plantings have since died. As the property owner, you are required to maintain the property in accordance with the approved site plan and the plantings that are no longer standing or in poor health must be replaced. Please make preparations to perform this work in the Spring.

Please respond to this letter as soon as possible so that you can present me with a plan to bring the property back in compliance with the approved site plan.

Please do not hesitate to contact me if you have any questions.

Sincerely,

Jim Marchese, Code Enforcement Officer

Cc: Mark Connors, Town Planner



TOWN OF STRATHAM

INCORPORATED 1716
10 BUNKER HILL AVENUE • STRATHAM NH 03885
VOICE (603) 772-7391 • FAX (603) 775-0517 • www.strathamnh.gov

May 2, 2022

Tulip Tree LLC
Attn: Sophie and Kyle Saltonstall
61 Stratham Heights Road
Stratham, NH 03885

RE: 61 Stratham Heights Road, Map 5, Lot 81
Planning Board Notice of Decision dated 3/22/2019

Dear Mr. and Ms. Saltonstall,

The purpose of this letter is to respond to an April 28, 2022 email from Kyle Saltonstall who was responding to a site visit by the office earlier in the day. The office appreciates Kyle's quick response to the questions raised during the site visit.

As stated in the 2/25/2022 letter you are required to maintain the property in accordance with the approved site plan by the Planning Board dated 3/22/2019. Spring of 2022 is now upon us. This office believes that a fair and reasonable expectation is to have the property in compliance by 6/15/2022. If your intention is not to regain compliance or to modify the approved plan you will need to file for an amendment to the site plan to the Planning Board by 6/15/2022. This will allow for the Planning Board to determine if your intended site modifications meet the goals established by the prior approval.

Please do not hesitate to contact me if you have any questions.

Sincerely,

Jim Marchese, Code Enforcement Officer

Cc: Mark Connors, Town Planner



**Town of Stratham, NH
Site Plan Review Application**

Map# 5 Lot# 81

Project Name: Amendment to Landscaping

Location: 61 STRATHAM HEIGHTS ROAD

Project Description: Please see attached letter + supporting documents

Zone: RA New Industrial / Commercial Square Footage: _____
or Number of Residential Units: _____

Applicant:

Name: SOPHIE SALTONSTALL Phone: 603 475 2144

Company: TULIP TREE LLC Fax: _____

Address: 61 STRATHAM HEIGHTS ROAD

Owner:

Name: SAME AS ABOVE Phone: _____

Company: _____ Fax: _____

Address: _____

Agent:

Contact Name: KYLE SALTONSTALL Phone: 603 312 4806

Company: SALTONSTALL FARM LLC Fax: _____

Address: 61 STRATHAM HEIGHTS ROAD

Email Address: KYLE@SALTONSTALLFARM.COM

By signing this application, you are agreeing to all rules and regulations of the Town of Stratham, and are agreeing to allow agents of the Town of Stratham to conduct inspections, during normal business hours, of your property, to ensure compliance with all Stratham Zoning and Site Review regulations while your application is under consideration and during any construction and operational phases after approval is granted.

The Signor shall be the owner or the signor shall provide a letter signed by all the property owners giving the signor permission to represent the owner in presentation of this application.

Signed: Sophie Saltonstall Date: 6-15-2022

Fees:

Notification Fee: \$150.00 plus Abutters Fee: 5 Abutters X \$8.00 = \$ 40.00

Site Review Fee*: \$ _____.00 One-Hundred (\$100) dollars for each 1,000 square feet of building construction – with a minimum of one-hundred (\$100) dollars.

Preliminary Consultation as provided for under Section 4.1, a filling fee of \$75.00.

Total Fees: \$ 190.00

See Section 4.2.7 of the Site Review Regulations for fee schedule.
*Additional fees may be charged to cover inspection and review cost.

For Office Use Only

Date Application Received: _____ Total Fees Collected with Application: \$ _____.00

Abutters List Received: _____ Check List Received: _____

PB Hearing Date: _____ Notice Date: _____ PB Jurisdiction Acceptance Date: _____

**S
i
t
e

R
e
v
i
e
w**

K, Saltenstall
ZBA
Abutters

~~BARR, DAVID P.
BARR, ANNE Y.
7 ORCHARD HILL ROAD
STRATHAM, NH 03885~~

BENEFIELD KUNOWSKI FAMILY
BENEFIELD, RICHARD D. -TR
KUNOWSKI, JOHN F. -TRUSTEE
55 STRATHAM HEIGHTS
STRATHAM, NH 03885

COOPER FAMILY REVOCABLE T
COOPER, JOSHUA M. & KIMBER
9 ORCHARD HILL ROAD
STRATHAM, NH 03885

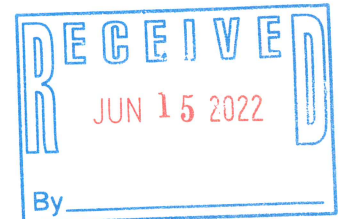
MCCARTHY, NINA
MCCARTHY, STEPHEN
54 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

MCDONNELL, MICHAEL C. & C
TRUSTEES OF MCDONNELL FAM
P.O. BOX 186
SEABROOK, NH 03874

TULIP TREE LLC
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

5 + 8
40
\$150 Base fee
\$190.00

K Kyle Saltonstall
61 Stratham Heights Road, Stratham, NH 03885
(603)312-4806
Kyle@SaltonstallFarm.com



Dear Stratham Planning Department and Planning Board,

I am writing to ask for an amendment to our current site plan for our property at 61 Stratham Heights Road. The site plan originally called for evergreen trees (highlighted in red) to be planted on the north side of our eco-parking area (highlighted in green).

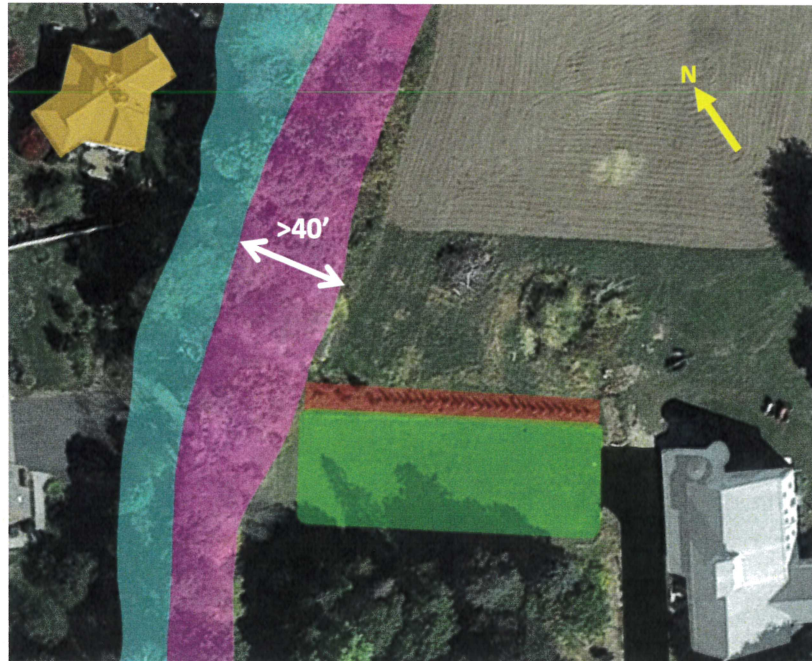


Mr. Marchese informed me via in-person meeting and official letter that we must either “maintain the property in accordance with the approved site plan” or “file for an amendment to the Planning Board.” Given no other alternatives, we have chosen to pursue the latter option for the following reasons:

- ...the insufficiency of the previously approved landscaping to achieve the intents of the original site plan and Stratham’s Site Plan Review Regulations,
- ...our ongoing consultation with a landscape architect and site plan engineer to develop a whole property design which will implement landscaping and property management stratagem that will far exceed the intent of the original site plan and Stratham’s Site Plan Review Regulations,
- ...and the significant costs associated with reimplementing the previously approved landscaping in question, knowing that the trees would likely not be permanent.

The insufficiency of the previously approved landscaping to achieve the intent of the original site plan and Stratham's Site Plan Review Regulations

The original intent behind placing evergreen trees along the north side of the eco-parking area was to shield the Cooper residence (highlighted in orange) from nuisance noise and light emitted from usage of our eco-parking area. Stratham's Site Plan Review Regulations Section 5.2-C-2 states that "Landscaping... must be provided with proper regard to adjacent properties... *including interior landscaping of parking areas.*" Because of this, the more than 40' of dense vegetation (highlighted in pink and blue) between properties was deemed inadequate and we added landscaping around our eco-parking area.



Unfortunately, after the trees required by the current site plan were planted in June of 2020, they did not fare well. This is despite using the best practices outlined in Stratham Site Plan Review Regulations Section 5.2-K, namely deep tillage and incorporation of organic matter. I suspect this is due to them being...

- ...located in an area generally inhospitable to evergreen trees. The eco-parking area is filled with several feet of exceptionally well drained gravel; the eco-parking area and its border are thus bone dry unless frequently irrigated. Frequent irrigation is not plausible during the winter months when our irrigation is shut off due to frozen ground conditions, yet evergreen trees still require soil moisture in those months.
- ...planted at a terrible time of year. June is possibly the worst time of year to plant evergreen trees, an activity best suited for the early spring as soon as soil is workable. Even fall or winter are more suitable times to plant evergreen trees, but the heat of summer will put incredible stresses on evergreen trees that they are not prepared for during transplant.
- ...killed by an unknown factor that I am unaware of. Plant pathology is something I'm still new to.

If my assumptions are correct and this area is poorly suited for evergreen trees, replanting this area with trees doomed to wither and die will not only fail the intent to shield our abutters from light or noise generated from the eco-parking area, but it will also fail Stratham's Site Plan Review Regulations Sections 5.2-C-5-b and 5.2-C-5-c, which state that "Plant varieties shall be selected to ensure long-term survival" and "Plant varieties shall be selected so as to reduce long-term maintenance." Furthermore, the trees as drawn on the site plan are drawn linearly along the edge of the eco-parking area, but Stratham's Site Plan Review Regulations Section 5.2-C-10 states that landscaping should "...be laid out in informal drifts rather than formal rows," and that "Linear solutions shall be avoided wherever possible." Finally, the approved plan did not, as is required by Stratham's Site Plan Review Regulations Section 5.2-C-16, "Use a variety of species to... Create visual depth in plant massing by layering plants of various textures, sizes and colors," or "Include flowering or fruiting species for color, interest, and wildlife habitat where appropriate."

Our ongoing consultation with a landscape architect and site plan engineer to develop a whole property management plan which will implement landscaping and property management stratagem that will far exceed the intent of the original site plan and Stratham's Site Plan Review Regulations

The previous plan was developed by a local engineering firm. While they are most certainly one of the best firms around for site planning and while we are 100% satisfied with their designs for the eco-parking area and related site drainage, we don't believe that an engineering firm is always the best choice for landscape design. Moving forward, we've chosen to enlist the services of a landscape architect to help us landscape much of our property on the north side of Stratham Heights Road.

I have attached a letter from Cornelius Murphy, the Principal Landscape Architect from Whole Systems Design Collective, LLC. Our conversations with Mr. Murphy have centered our values in property management, which include soil health regeneration, sustainable water use & reduction of run-off, creation of wildlife habitat, and beautification of the property. We have also stressed the need for landscaping to create a visual screen and noise buffer between our property and our abutters.

Although the process of generating a high quality property management plan can be frustratingly slow, especially during COVID times, we feel confident that the wait will be worth it. If we take the time to create a well-designed plan now, we believe that we can create a very special space that benefits our town far into the future. We also believe that the only way to do this correctly is by enlisting the aid of seasoned experts who are able to correctly interpret pertinent regulations while also designing a durable system that meets our needs.

The significant costs associated with reimplementing the previously approved landscaping in question, knowing that the trees would not be permanent

Twenty-five 6' evergreen trees at wholesale costs will likely amount to over \$3,000 in trees alone. Add to this the cost of irrigation lines, soil amendments, and the labor to plant the trees, and I feel confident that my out-of-pocket costs would be greater than \$5,000. If I believed that these trees would survive a summer planting and would meet the intent of the original site plan, \$5,000 would clearly be a reasonable cost. However, I believe that this would be a \$5,000 expenditure that would fail to screen our eco-parking area from our abutters, fail to meet the requirements set forth in Stratham's Site Plan Review Regulations, and would genuinely be an unnecessary economic stress on one of Stratham's few remaining agricultural operations.

Our Proposal for a Site Plan Amendment

Seeing as we are currently working on a superior plan with long term durability – yet are still very interested in meeting the intent of the original site plan in the meantime – we are asking for the approval of the Planning Board to amend the site plan to allow us to continue maintaining the area in question using a more “farm-ish” practice. The area immediately north of our eco-parking area is currently growing eight closely spaced rows of sweet corn. The plants are well on track to meet the adage of “knee-high by the Fourth of July,” and are well fertilized and well irrigated. Anyone who has visited the Scamman Farm’s corn maze knows how effective a corn wall can be at screening sound and light, and we have planted the area far thicker than the walls in the Scamman Farm corn maze. We expect that our corn will be well over head height by the time that our season gets busy in September and October.

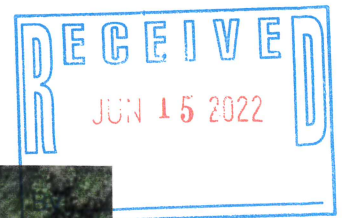
We are currently expecting our second child to be born any day now and have worked to keep our schedule clear of any responsibilities for the summer. Knowing this, the only use of our barn, and thus the eco-parking area, will be for a community Yoga class on Mondays. The instructor uses a novel system to instruct her students; they all wear headphones tied into her microphone. It is quite literally silent, save for the gently exhalations of the yogis. The Yoga class is over by 7 pm, well before sunset. Although the corn barrier will still be growing to its maximum height during these months and will not provide a complete screen, we believe that it will provide a better screen and be less disruptive to our abutters than tilling the soil, amending the soil, and planting twenty-five trees.

This is clearly an ongoing conversation between my family, our abutters, and the Town of Stratham. To best assist Stratham’s Planning Department and Planning Board in helping us make decisions which will best improve our neighborhood and our town, we have included maps and photographs which show the physical locations and conditions of the areas in question. While we understand that sometimes no simple solutions can be found, we hope that given enough time, all parties can come to a compromise that will allow us to exist and thrive as healthy and active participants in our community.

Very Respectfully,



K Kyle Saltonstall



This map is included for the purpose of understanding changes to the property since the satellite images were taken in October of 2021.

- The area highlighted in red shows twenty-five 6' arborvitae trees. These trees died and are no longer in this area.
- The areas highlighted in blue shows a large mound of soil displaced during the construction of our parking area and a brush pile. Both the dirt and the brush have been removed and this area is currently covered in a lush field of clover and fescue.
- The area highlighted in green shows a bare dirt field. This area is currently planted in a cover crop consisting of phacelia, marigold, and cosmos, plant species selected for their ability to improve soil conditions and offer beauty as they bloom throughout the summer and fall.



This map is included to show the cone of noise emitted from both barn doors when fully open. Highlighted in orange is the Cooper Residence.



This map show the locations and directions from which the photos on the following three pages were taken.



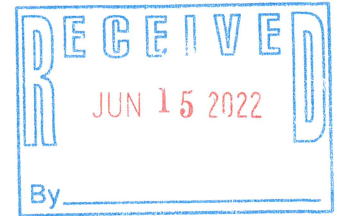
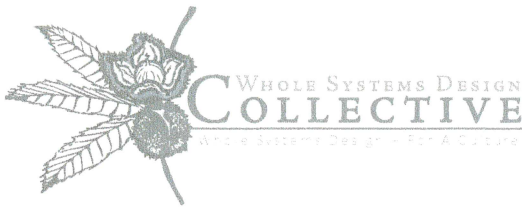
Blue arrow, taken on June 15, 2022 from the center of the eco-parking area, facing directly at the Cooper Residence.



Red arrow, taken on June 15, 2022 from our lawn, facing directly at the Cooper Residence.



Green arrow, taken from our rear barn door, facing directly at the Cooper Residence.



Stratham Planning Board
Stratham Municipal Center
10 Bunker Hill Avenue
Stratham, NH 03885

June 14, 2022

Dear Stratham Planning Board Members,

We are writing to express our support for Saltonstall Farm regarding their efforts to mitigate noise and viewshed influences on neighboring properties and the concerns over a previously proposed planting buffer. Kyle & Sophie have recently engaged our firm to provide them with a comprehensive landscape plan for the area surrounding the north side of the event barn, an area that includes the planting buffer in question. We are in the early phases of design but hope to have a plan in place by the fall that would address any and all of the outstanding concerns but in a way that better meets the needs of the farm compared to the previously proposed planting.

We understand the alternative to waiting for our work to be complete would be to plant a stand of evergreen trees this summer. While we can appreciate the intent of a quick resolution, the plantings are much more likely to fail given the time of year which would leave the issues still unresolved. We are advocating for some time to consider the appropriateness of the proposed buffer in relationship to the other needs of the farm, the opportunity to entertain alternatives, and (at a minimum) the ability to delay any evergreen planting(s) till the fall to increase the chances of success.

Thank you for your time and consideration. Please let us know if there is anything else we can do to assist in this matter

Cheers,

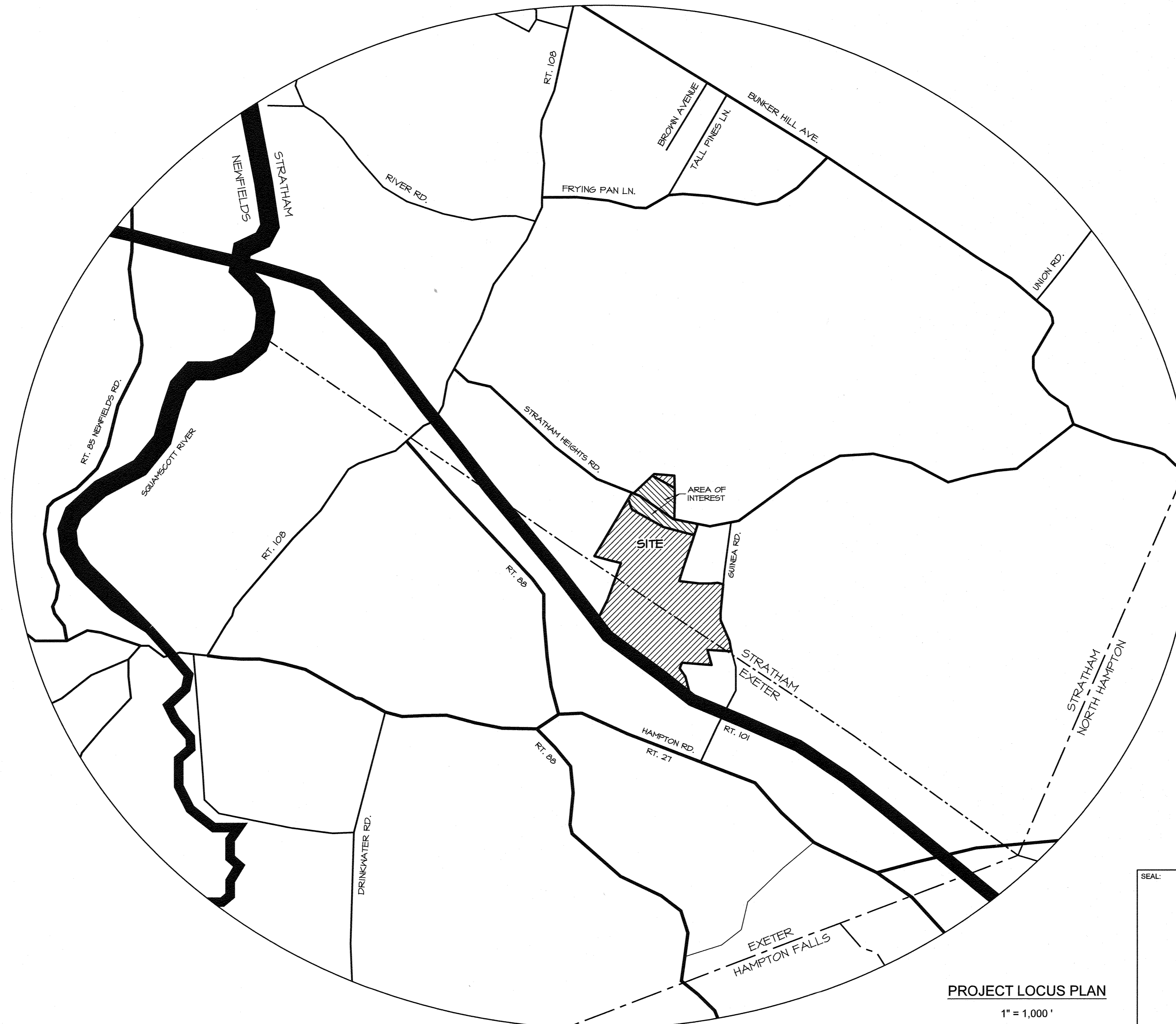
Cornelius Murphy // RLA
Principal and Co-Founder
Whole Systems Design Collective, LLC

SOPHIE ROBINSON
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

LAND SURVEYOR
RICHARD PARKER
146 DRINKWATER ROAD
KENSINGTON, NH 03833

AMY DUTTON HOME
9 WALKER STREET
KITTERY, ME 03904

STRATHAM, NH 03885



	COVER SHEET
1461-2	PLAT OF LAND FOR DR. HENRY SALTONSTALL
C1	EXISTING CONDITIONS
C2A	SITE PLAN
C2B	SITE PLAN
C3	GRADING & DRAINAGE PLAN
D1 & D2	NOTES
D3 & D4	DETAILS
A12	ARCHITECTURAL ELEVATION

3	JULY 9, 2019	FOR APPROVAL	
2	FEB 28, 2019	FOR APPROVAL	
1	JAN 16, 2019	FOR APPROVAL	
ISS. DATE:		DESCRIPTION OF ISSUE:	CHK.
DRAWN: JJM		DESIGN: JJM	
CHECKED: BDS		CHECKED: BDS	

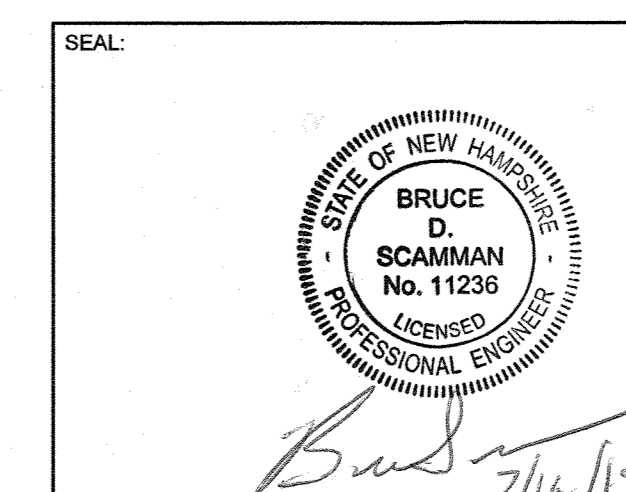


CLIENT: SOPHIE ROBINSON
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

TITLE:

COVER SHEET
FOR
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

PROJECT: 18-150	SCALE: AS SHOWN	SHEET: COVER
--------------------	--------------------	-----------------



SCALE 1"=200' NOVEMBER 1996

PARKER SURVEY ASSOC., INC.
13 HAMPTON RD. EXETER, N.H.

* DENOTES CORRECTIONS

NOTES

1. NO FIELD WORK DONE BY PARKER SURVEY AT THIS TIME. PLAN DERIVED FROM "PLAT OF LAND FOR DR. HENRY SALTONSTALL IN STRATHAM & EXETER, N.H. - SCALE: 1"=200'- MARCH 1973- REVISED DEC. 1986- PARKER SURVEY ASSOC., INC." PSA #1461
2. SEE "PLAT OF LAND FOR DR. HENRY SALTONSTALL TO BE CONVEYED TO WILLIAM L. WHITE IN STRATHAM, N.H. - SCALE: 1"=40'- FEB. 1973- PARKER SURVEY ASSOC., INC." PSA #1444
3. SEE "DRURY PLAINS SUBDIVISION, STRATHAM, N.H. - SCALE: 1"=40'- DATE: 1/15/86- KIMBALL CHASE COMPANY INC." RCR JD-14569

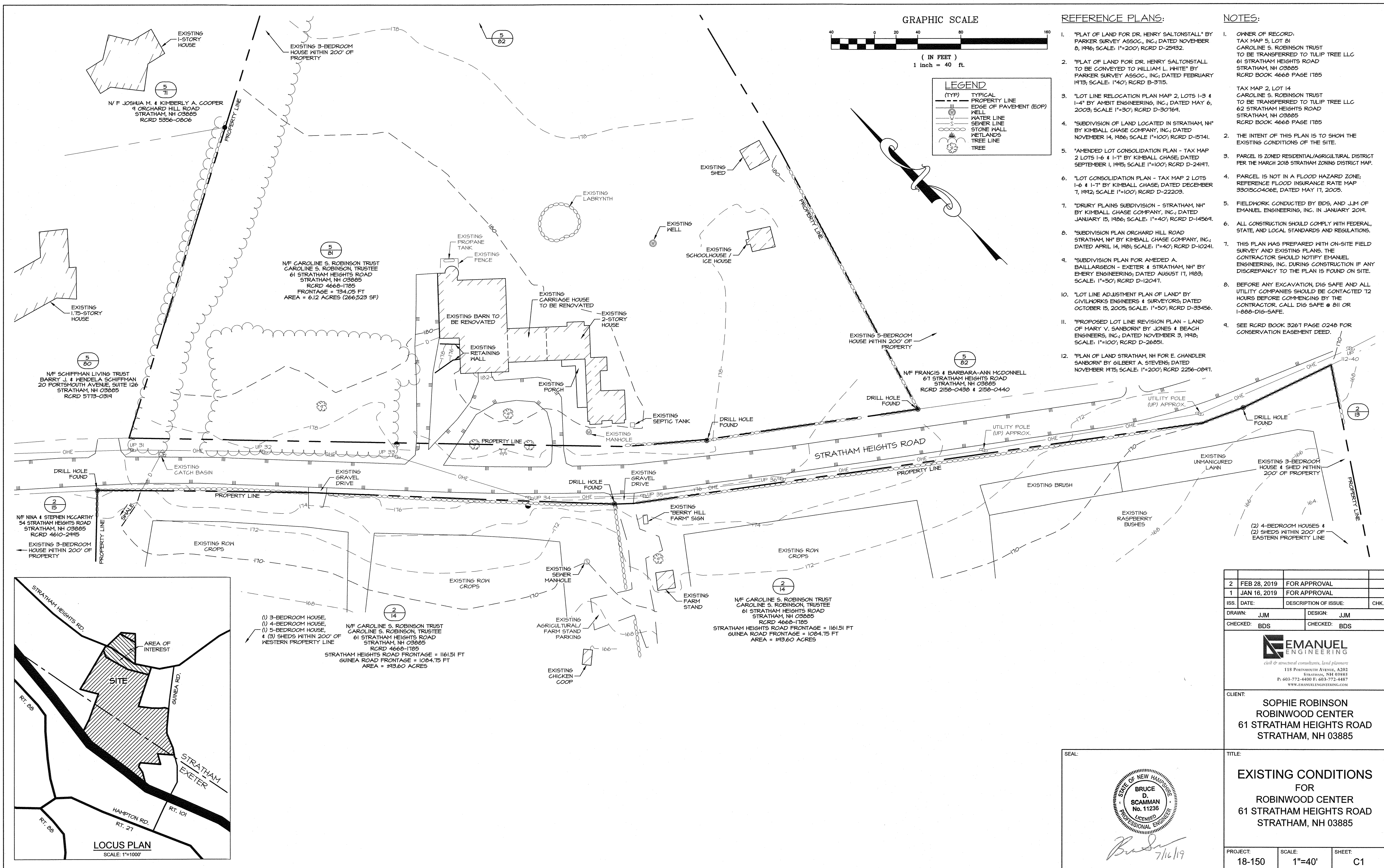


D-25932

I CERTIFY THAT THIS SURVEY PLAT IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN.

NOVEMBER 8, 1996
DATE

RICHARD K. PARKER JR. LLS #



REFERENCE PLANS:

- "PLAT OF LAND FOR DR. HENRY SALTONSTALL" BY PARKER SURVEY ASSOC., INC.; DATED NOVEMBER 8, 1946; SCALE: 1"=200'; RCRD D-25432.
- "PLAT OF LAND FOR DR. HENRY SALTONSTALL TO BE CONVEYED TO WILLIAM L. WHITE" BY PARKER SURVEY ASSOC., INC.; DATED FEBRUARY 1913; SCALE: 1"=40'; RCRD B-3715.
- "LOT LINE RELOCATION PLAN MAP 2, LOTS 1-3 & 1-4" BY AMBIT ENGINEERING, INC.; DATED MAY 6, 2003; SCALE 1"=30'; RCRD D-30164.
- "SUBDIVISION OF LAND LOCATED IN STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC.; DATED NOVEMBER 14, 1986; SCALE 1"=100'; RCRD D-15741.
- "AMENDED LOT CONSOLIDATION PLAN - TAX MAP 2 LOTS 1-6 & 1-7" BY KIMBALL CHASE, DATED SEPTEMBER 1, 1995; SCALE 1"=100'; RCRD D-24191.
- "LOT CONSOLIDATION PLAN - TAX MAP 2 LOTS 1-6 & 1-7" BY KIMBALL CHASE, DATED DECEMBER 7, 1992; SCALE 1"=100'; RCRD D-22203.
- "DRURY PLAINS SUBDIVISION - STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC.; DATED JANUARY 15, 1986; SCALE: 1"=40'; RCRD D-14564.
- "SUBDIVISION PLAN ORCHARD HILL ROAD STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC.; DATED APRIL 14, 1981; SCALE: 1"=40'; RCRD D-10241.
- "SUBDIVISION PLAN FOR AMENDED A. BAILLARGEON - EXETER & STRATHAM, NH" BY EMMER ENGINEERING, DATED AUGUST 17, 1983; SCALE: 1"=50'; RCRD D-12047.
- "LOT LINE ADJUSTMENT PLAN OF LAND" BY CIVILWORKS ENGINEERS & SURVEYORS; DATED OCTOBER 15, 2005; SCALE: 1"=50'; RCRD D-33456.
- "PROPOSED LOT LINE REVISION PLAN - LAND OF MARY V. SANBORN" BY JONES & BEACH ENGINEERS, INC.; DATED NOVEMBER 3, 1990; SCALE: 1"=100'; RCRD D-26051.
- "PLAN OF LAND STRATHAM, NH FOR E. CHANDLER SANBORN" BY GILBERT A. STEVENS; DATED NOVEMBER 1975; SCALE: 1"=200'; RCRD 2256-0847.

NOTES:

- OWNER OF RECORD: TAX MAP 5, LOT 81 CAROLINE S. ROBINSON TRUST TO BE TRANSFERRED TO TULIP TREE LLC 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885 RCRD BOOK 4668 PAGE 1785
- TAX MAP 2, LOT 14 CAROLINE S. ROBINSON TRUST TO BE TRANSFERRED TO TULIP TREE LLC 62 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885 RCRD BOOK 4668 PAGE 1785
- THE INTENT OF THIS PLAN IS TO SHOW THE EXISTING CONDITIONS OF THE SITE.
- PARCEL IS ZONED RESIDENTIAL/AGRICULTURAL DISTRICT PER THE MARCH 2018 STRATHAM ZONING DISTRICT MAP.
- PARCEL IS NOT IN A FLOOD HAZARD ZONE; REFERENCE FLOOD INSURANCE RATE MAP 33015C0406E, DATED MAY 17, 2005.
- FIELDWORK CONDUCTED BY BDS, AND JIM OF EMANUEL ENGINEERING, INC. IN JANUARY 2019.
- ALL CONSTRUCTION SHOULD COMPLY WITH FEDERAL, STATE, AND LOCAL STANDARDS AND REGULATIONS.
- THIS PLAN WAS PREPARED WITH ON-SITE FIELD SURVEY AND EXISTING PLANS. THE CONTRACTOR SHOULD NOTIFY EMANUEL ENGINEERING, INC. DURING CONSTRUCTION IF ANY DISCREPANCY TO THE PLAN IS FOUND ON SITE.
- BEFORE ANY EXCAVATION, DIG SAFE AND ALL UTILITY COMPANIES SHOULD BE CONTACTED 72 HOURS BEFORE COMMENCING BY THE CONTRACTOR. CALL DIG SAFE @ 811 OR 1-888-DIG-SAFE.
- SEE RCRD BOOK 3267 PAGE 0248 FOR CONSERVATION EASEMENT DEED.

2	FEB 28, 2019	FOR APPROVAL	
1	JAN 16, 2019	FOR APPROVAL	
ISS. DATE:	DESCRIPTION OF ISSUE:		CHK:
DRAWN: JUM	DESIGN: JUM		
CHECKED: BDS	CHECKED: BDS		

civil & structural consultants, land planners
118 PORTSMOUTH AVENUE, A202
STRATHAM, NH 03885
P: 603-772-4400 F: 603-772-4487
WWW.EMANUELENGINEERING.COM

CLIENT:

SOPHIE ROBINSON
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

TITLE:

EXISTING CONDITIONS
FOR
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

PROJECT:

18-150

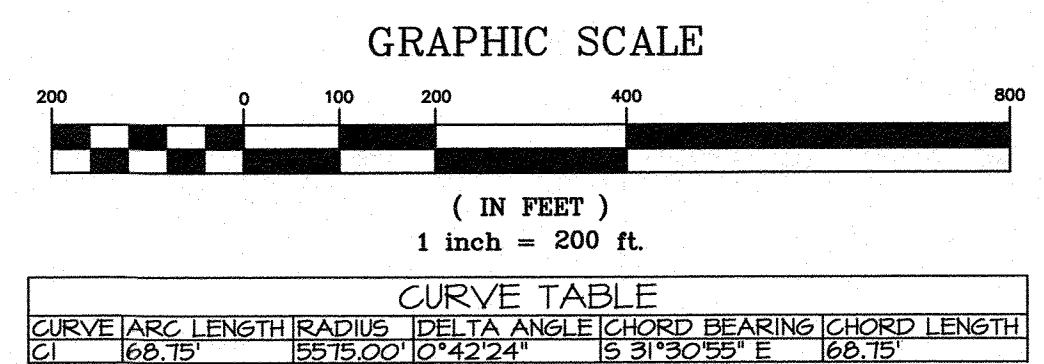
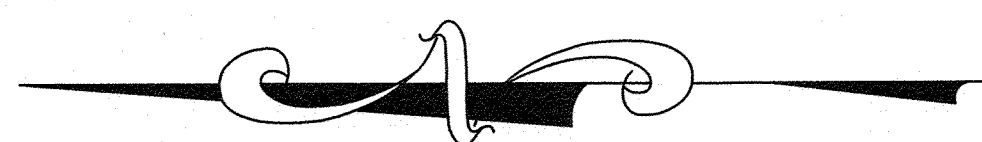
SCALE:

1"=40'

SHEET:

C1

SEAL:



LEGEND

(TYP) TYPICAL

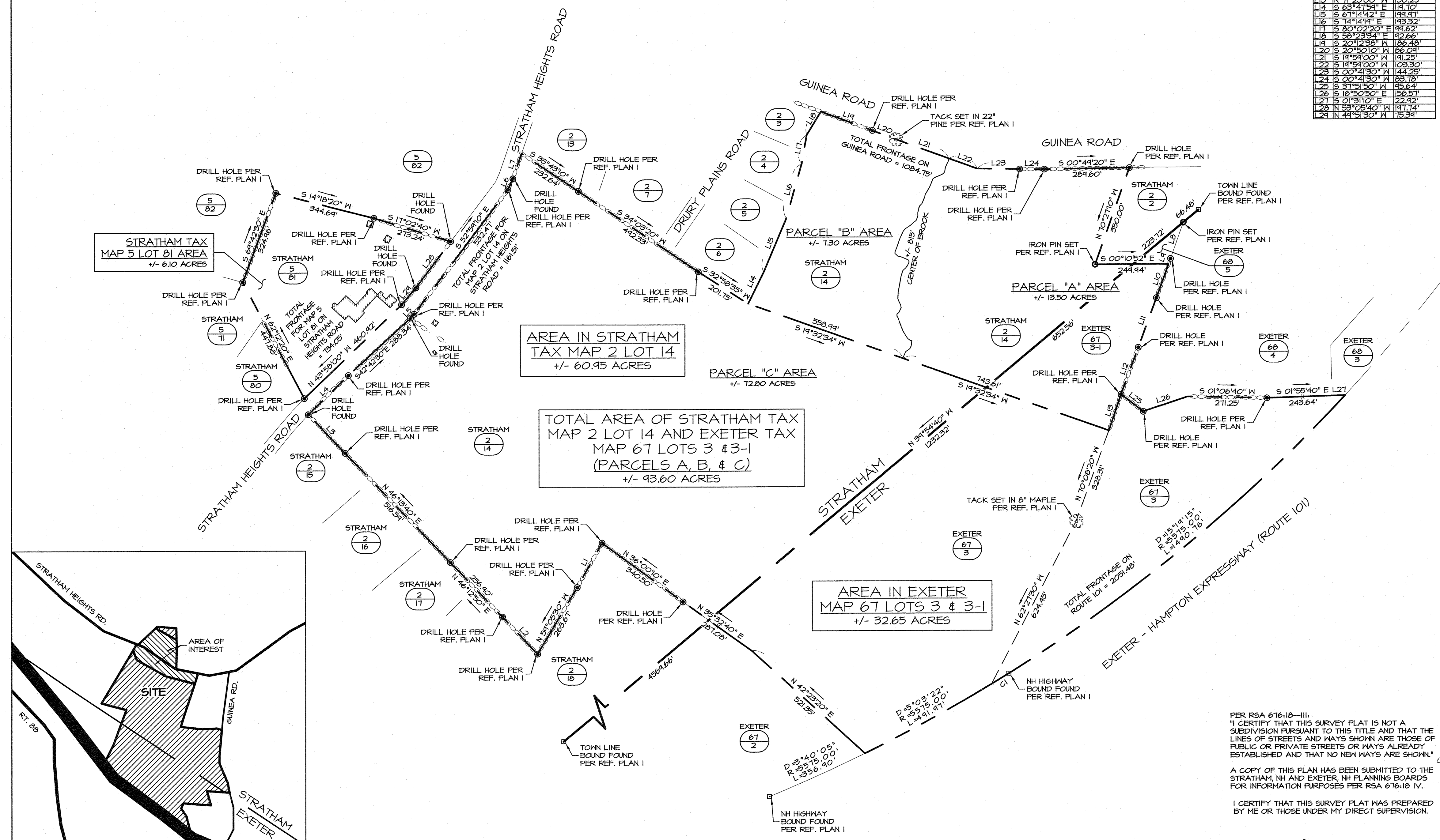
	PROPERTY LINE
	EDGE OF PAVEMENT (EOP)
	WELL
	WATER LINE
	SEWER LINE
	STONE WALL
	WETLANDS
	TREE LINE
	TREE

LINE TABLE

LINE	BEARING	DISTANCE
L1	N 60°45'10" W	172.64'
L2	N 46°44'00" E	175.83'
L3	N 48°14'50" E	181.44'
L4	S 44°25'50" E	191.56'
L5	S 46°54'10" E	177.68'
L6	S 68°23'50" E	40.74'
L7	S 10°54'00" E	120.12'
L8	N 10°27'10" W	131.03'
L9	N 10°15'20" W	22.85'
L10	N 10°15'20" W	122.43'
L11	N 64°51'20" W	114.48'
L12	N 64°48'00" W	110.12'
L13	N 11°23'00" W	150.23'
L14	S 63°41'54" E	114.10'
L15	S 61°14'42" E	144.47'
L16	S 14°14'18" E	163.32'
L17	S 20°02'20" E	94.62'
L18	S 58°23'34" E	127.66'
L19	S 20°15'50" W	106.48'
L20	S 20°50'10" W	106.04'
L21	S 14°54'00" W	141.25'
L22	S 14°11'00" W	103.30'
L23	S 00°41'30" W	144.29'
L24	S 00°41'30" W	153.16'
L25	S 17°51'50" W	155.64'
L26	S 18°50'50" E	159.51'
L27	S 01°31'10" E	22.42'
L28	N 53°05'40" W	141.74'
L29	N 44°51'50" W	119.34'

- REFERENCE PLANS:
- "PLAT OF LAND FOR DR. HENRY SALTONSTALL" BY PARKER SURVEY ASSOC., INC., DATED NOVEMBER 8, 1996; SCALE: 1"=200'; RCRD D-25432.
 - "PLAT OF LAND FOR DR. HENRY SALTONSTALL TO BE CONVEYED TO WILLIAM L. WHITE" BY PARKER SURVEY ASSOC., INC., DATED FEBRUARY 1973; SCALE: 1"=40'; RCRD B-3715.
 - "LOT LINE RELOCATION PLAN MAP 2, LOTS 1-3 & 1-4" BY AMBIT ENGINEERS, INC., DATED MAY 6, 2003; SCALE: 1"=30'; RCRD D-30764.
 - "SUBDIVISION OF LAND LOCATED IN STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC., DATED NOVEMBER 14, 1986; SCALE: 1"=100'; RCRD D-15741.
 - "AMENDED LOT CONSOLIDATION PLAN - TAX MAP 2 LOTS 1-6 & 1-7" BY KIMBALL CHASE, DATED SEPTEMBER 1, 1995; SCALE: 1"=100'; RCRD D-24197.
 - "LOT CONSOLIDATION PLAN - TAX MAP 2 LOTS 1-6 & 1-7" BY KIMBALL CHASE, DATED DECEMBER 1, 1995; SCALE: 1"=100'; RCRD D-22203.
 - "DRURY PLAINS SUBDIVISION - STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC., DATED JANUARY 15, 1986; SCALE: 1"=40'; RCRD D-14564.
 - "SUBDIVISION PLAN ORCHARD HILL ROAD STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC., DATED APRIL 14, 1981; SCALE: 1"=40'; RCRD D-10241.
 - "SUBDIVISION PLAN FOR AMEDEO A. BAILLARGEON - EXETER & STRATHAM, NH" BY EBERY ENGINEERS, DATED AUGUST 17, 1983; SCALE: 1"=50'; RCRD D-12047.
 - "LOT LINE ADJUSTMENT PLAN OF LAND" BY CIVILWORKS ENGINEERS & SURVEYORS, DATED OCTOBER 15, 2005; SCALE: 1"=50'; RCRD D-38456.
 - "PROPOSED LOT LINE REVISION PLAN - LAND OF MARTY V. SANBORN" BY JONES & BEACH ENGINEERS, INC., DATED NOVEMBER 3, 1998; SCALE: 1"=100'; RCRD D-26851.
 - "PLAN OF LAND STRATHAM, NH FOR E. CHANDLER SANBORN" BY GILBERT A. STEVENS, DATED NOVEMBER 1975; SCALE: 1"=200'; RCRD 2256-0897.

- NOTES:
- OWNER OF RECORD: STRATHAM TAX MAP 5, LOT 81 TULIP TREE LLC 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885 RCRD BOOK 6005 PAGE 2412
 - STRATHAM TAX MAP 2, LOT 14 & EXETER TAX MAP 67 LOT 3 TULIP TREE LLC 62 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885 RCRD BOOK 6005 PAGE 2412
 - THE INTENT OF THIS PLAN IS TO SHOW THE BOUNDARY LINES FOR THE SITE.
 - PARCEL IS ZONED RESIDENTIAL/AGRICULTURAL DISTRICT PER THE MARCH 2018 STRATHAM ZONING DISTRICT MAP.
 - PARCEL IS NOT IN A FLOOD HAZARD ZONE; REFERENCE FLOOD INSURANCE RATE MAP 3805C0406E, DATED MAY 17, 2005.
 - FIELDWORK CONDUCTED BY BDS, AND JJM OF EMANUEL ENGINEERING, INC. IN JANUARY 2019.
 - THIS PLAN WAS PREPARED WITH ON-SITE FIELD SURVEY AND REFERENCE PLAN #1. THE CONTRACTOR SHOULD NOTIFY EMANUEL ENGINEERING, INC. DURING CONSTRUCTION IF ANY DISCREPANCY TO THE PLAN IS FOUND ON SITE.
 - SEE RCRD BOOK 3267 PAGE 0248 FOR CONSERVATION EASEMENT DEED.



PER RSA 676:18-III:
"I CERTIFY THAT THIS SURVEY PLAT IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN."

A COPY OF THIS PLAN HAS BEEN SUBMITTED TO THE STRATHAM, NH AND EXETER, NH PLANNING BOARDS FOR INFORMATION PURPOSES PER RSA 676:18 IV.

I CERTIFY THAT THIS SURVEY PLAT WAS PREPARED BY ME OR THOSE UNDER MY DIRECT SUPERVISION.

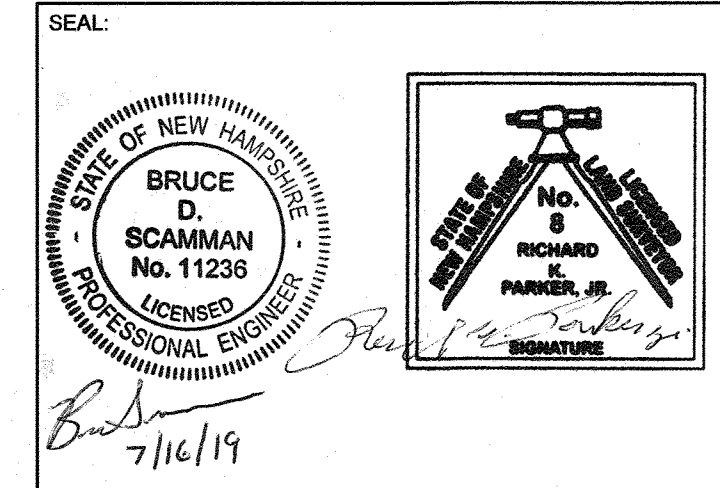
THIS PLAN IS A RESULT OF FIELD SURVEY PERFORMED BY BDS AND JJM OF EEI IN JANUARY 2019. THE ERROR OF CLOSURE IS BETTER THAN 1/10,000. SURVEY PER NHLSA STANDARDS, CATEGORY I, CONDITION I. AREA CALCULATIONS ARE PER BENCHMARK SOFTWARE.

APPROVED BY THE TOWN OF STRATHAM PLANNING BOARD

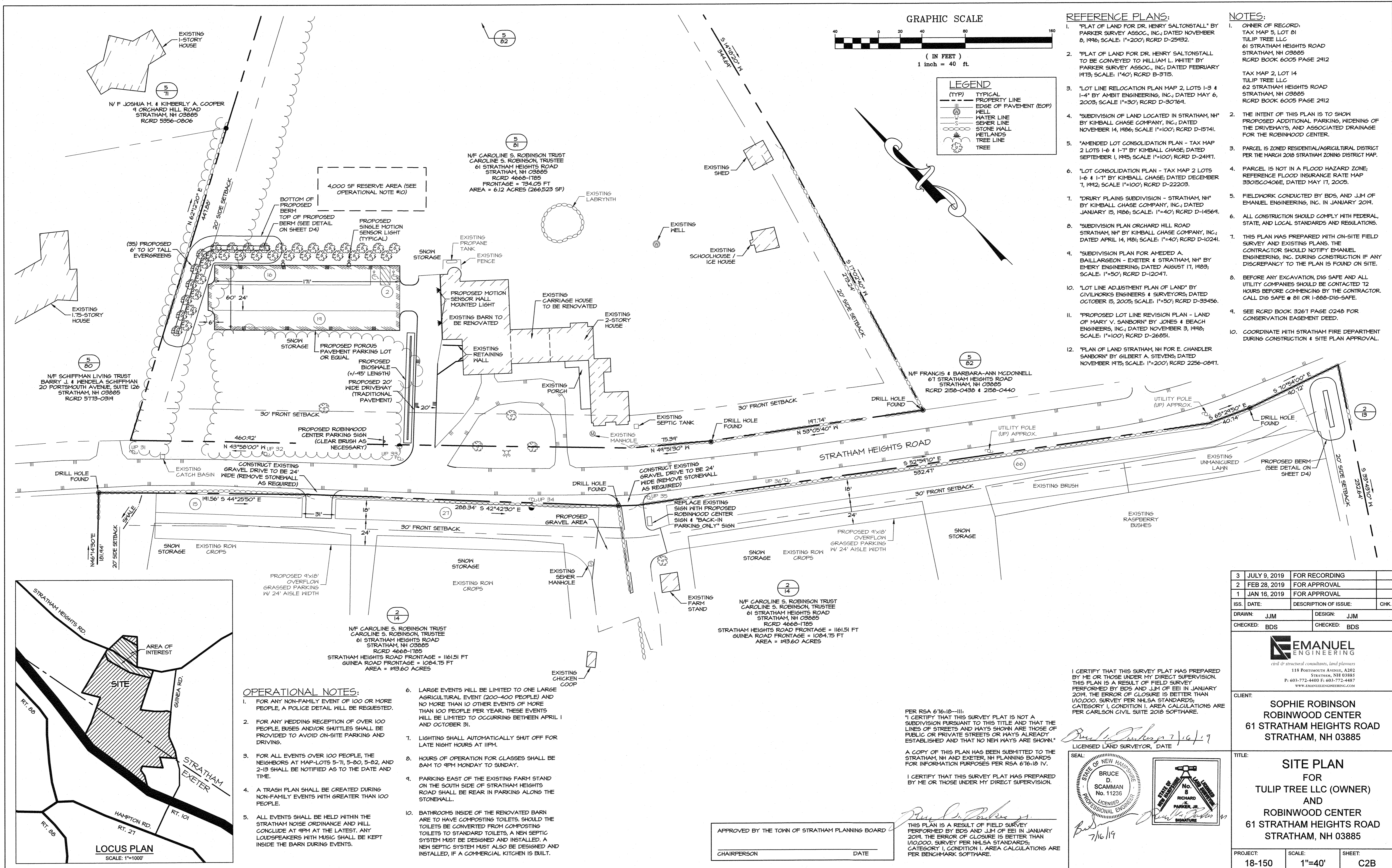
CHAIRPERSON _____ DATE _____

I CERTIFY THAT THIS SURVEY PLAT WAS PREPARED BY ME OR THOSE UNDER MY DIRECT SUPERVISION. THIS PLAN IS A RESULT OF FIELD SURVEY PERFORMED BY BDS AND JJM OF EEI IN JANUARY 2019. THE ERROR OF CLOSURE IS BETTER THAN 1/10,000. SURVEY PER NHLSA STANDARDS, CATEGORY I, CONDITION I. AREA CALCULATIONS ARE PER CARLSON CIVIL SUITE 2018 SOFTWARE.

Bruce D. Scamman 7/16/19
LICENSED LAND SURVEYOR, DATE



1	JULY 9, 2019	FOR RECORDING	
ISS. DATE:	DESCRIPTION OF ISSUE:		CHK.
DRAWN: JJM	DESIGN: JJM		
CHECKED: BDS	CHECKED: BDS		
CLIENT: SOPHIE ROBINSON ROBINWOOD CENTER 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885			
TITLE: SITE PLAN FOR TULIP TREE LLC (OWNER) AND ROBINWOOD CENTER 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885			
PROJECT:	SCALE:	SHEET:	
18-150	1"=40'	C2A	



- REFERENCE PLANS:**
- "PLAT OF LAND FOR DR. HENRY SALTONSTALL" BY PARKER SURVEY ASSOC., INC.; DATED NOVEMBER 8, 1946; SCALE: 1"=200'; RCRD D-25432.
 - "PLAT OF LAND FOR DR. HENRY SALTONSTALL TO BE CONVEYED TO WILLIAM L. WHITE" BY PARKER SURVEY ASSOC., INC.; DATED FEBRUARY 1973; SCALE: 1"=40'; RCRD B-3715.
 - "LOT LINE RELOCATION PLAN MAP 2, LOTS 1-3 & 1-4" BY AMBIT ENGINEERING, INC.; DATED MAY 6, 2003; SCALE: 1"=30'; RCRD D-30764.
 - "SUBDIVISION OF LAND LOCATED IN STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC.; DATED NOVEMBER 14, 1986; SCALE: 1"=100'; RCRD D-15741.
 - "AMENDED LOT CONSOLIDATION PLAN - TAX MAP 2 LOTS 1-6 & 1-7" BY KIMBALL CHASE, DATED SEPTEMBER 1, 1945; SCALE: 1"=100'; RCRD D-24147.
 - "LOT CONSOLIDATION PLAN - TAX MAP 2 LOTS 1-6 & 1-7" BY KIMBALL CHASE; DATED DECEMBER 7, 1942; SCALE: 1"=100'; RCRD D-22203.
 - "DRURY PLAINS SUBDIVISION - STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC.; DATED JANUARY 15, 1986; SCALE: 1"=40'; RCRD D-14564.
 - "SUBDIVISION PLAN ORCHARD HILL ROAD STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC.; DATED APRIL 14, 1981; SCALE: 1"=40'; RCRD D-10241.
 - "SUBDIVISION PLAN FOR AMENDED A. BAILLARGEON - EXETER & STRATHAM, NH" BY EMERY ENGINEERS, DATED AUGUST 17, 1983; SCALE: 1"=50'; RCRD D-12047.
 - "LOT LINE ADJUSTMENT PLAN OF LAND" BY CIVILWORKS ENGINEERS & SURVEYORS; DATED OCTOBER 15, 2005; SCALE: 1"=50'; RCRD D-33456.
 - "PROPOSED LOT LINE REVISION PLAN - LAND OF HARRY V. SANBORN" BY JONES & BEACH ENGINEERS, INC.; DATED NOVEMBER 3, 1946; SCALE: 1"=100'; RCRD D-26851.
 - "PLAN OF LAND STRATHAM, NH FOR E. CHANDLER SANBORN" BY GILBERT A. STEVENS; DATED NOVEMBER 1975; SCALE: 1"=200'; RCRD 2256-0897.

- NOTES:**
- OWNER OF RECORD: TAX MAP 5, LOT 81 TULIP TREE LLC 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885 RCRD BOOK 6005 PAGE 2412
 - TAX MAP 2, LOT 14 TULIP TREE LLC 62 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885 RCRD BOOK 6005 PAGE 2412
 - THE INTENT OF THIS PLAN IS TO SHOW PROPOSED ADDITIONAL PARKING, WIDENING OF THE DRIVEWAYS, AND ASSOCIATED DRAINAGE FOR THE ROBINWOOD CENTER.
 - PARCEL IS ZONED RESIDENTIAL/AGRICULTURAL DISTRICT PER THE MARCH 2018 STRATHAM ZONING DISTRICT MAP.
 - PARCEL IS NOT IN A FLOOD HAZARD ZONE; REFERENCE FLOOD INSURANCE RATE MAP 33015C0406E, DATED MAY 17, 2005.
 - FIELDWORK CONDUCTED BY BDS AND JIM OF EMANUEL ENGINEERING, INC. IN JANUARY 2019.
 - ALL CONSTRUCTION SHOULD COMPLY WITH FEDERAL, STATE, AND LOCAL STANDARDS AND REGULATIONS.
 - THIS PLAN WAS PREPARED WITH ON-SITE FIELD SURVEY AND EXISTING PLANS. THE CONTRACTOR SHOULD NOTIFY EMANUEL ENGINEERING, INC. DURING CONSTRUCTION IF ANY DISCREPANCY TO THE PLAN IS FOUND ON SITE.
 - BEFORE ANY EXCAVATION, DIG SAFE AND ALL UTILITY COMPANIES SHOULD BE CONTACTED 12 HOURS BEFORE COMMENCING BY THE CONTRACTOR. CALL DIG SAFE @ 811 OR 1-888-DIG-SAFE.
 - SEE RCRD BOOK 3267 PAGE 0248 FOR CONSERVATION EASEMENT DEED.
 - COORDINATE WITH STRATHAM FIRE DEPARTMENT DURING CONSTRUCTION & SITE PLAN APPROVAL.

OPERATIONAL NOTES:

- FOR ANY NON-FAMILY EVENT OF 100 OR MORE PEOPLE, A POLICE DETAIL WILL BE REQUESTED.
- FOR ANY WEDDING RECEPTION OF OVER 100 PEOPLE, BUSES AND/OR SHUTTLES SHALL BE PROVIDED TO AVOID ON-SITE PARKING AND DRIVING.
- FOR ALL EVENTS OVER 100 PEOPLE, THE NEIGHBORS AT MAP-LOTS 5-71, 5-80, 5-82, AND 2-13 SHALL BE NOTIFIED AS TO THE DATE AND TIME.
- A TRASH PLAN SHALL BE CREATED DURING NON-FAMILY EVENTS WITH GREATER THAN 100 PEOPLE.
- ALL EVENTS SHALL BE HELD WITHIN THE STRATHAM NOISE ORDINANCE AND WILL CONCLUDE AT 9PM AT THE LATEST. ANY LOUDSPEAKERS WITH MUSIC SHALL BE KEPT INSIDE THE BARN DURING EVENTS.
- LARGE EVENTS WILL BE LIMITED TO ONE LARGE AGRICULTURAL EVENT (200-400 PEOPLE) AND NO MORE THAN 10 OTHER EVENTS OF MORE THAN 100 PEOPLE PER YEAR. THESE EVENTS WILL BE LIMITED TO OCCURRING BETWEEN APRIL 1 AND OCTOBER 31.
- LIGHTING SHALL AUTOMATICALLY SHUT OFF FOR LATE NIGHT HOURS AT 11PM.
- HOURS OF OPERATION FOR CLASSES SHALL BE 8AM TO 4PM MONDAY TO SUNDAY.
- PARKING EAST OF THE EXISTING FARM STAND ON THE SOUTH SIDE OF STRATHAM HEIGHTS ROAD SHALL BE REAR IN PARKING ALONG THE STONEWALL.
- BATHROOMS INSIDE OF THE RENOVATED BARN ARE TO HAVE COMPOSTING TOILETS. SHOULD THE TOILETS BE CONVERTED FROM COMPOSTING TOILETS TO STANDARD TOILETS, A NEW SEPTIC SYSTEM MUST BE DESIGNED AND INSTALLED. A NEW SEPTIC SYSTEM MUST ALSO BE DESIGNED AND INSTALLED, IF A COMMERCIAL KITCHEN IS BUILT.

PER RSA 676:10-III:
"I CERTIFY THAT THIS SURVEY PLAT IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN."

A COPY OF THIS PLAN HAS BEEN SUBMITTED TO THE STRATHAM, NH AND EXETER, NH PLANNING BOARDS FOR INFORMATION PURPOSES PER RSA 676:10 IV.

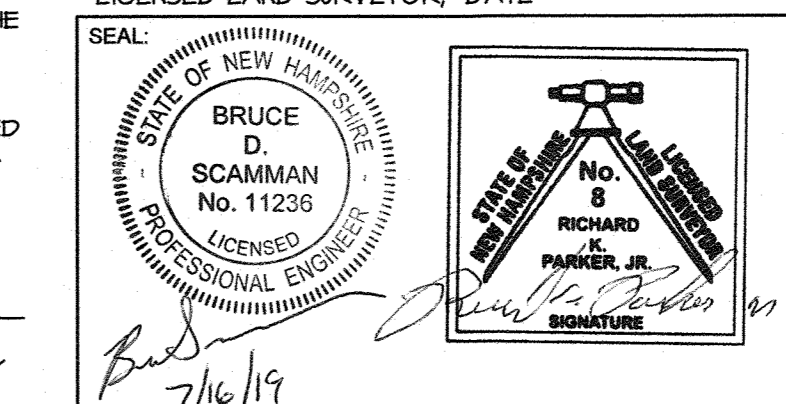
I CERTIFY THAT THIS SURVEY PLAT WAS PREPARED BY ME OR THOSE UNDER MY DIRECT SUPERVISION.

APPROVED BY THE TOWN OF STRATHAM PLANNING BOARD

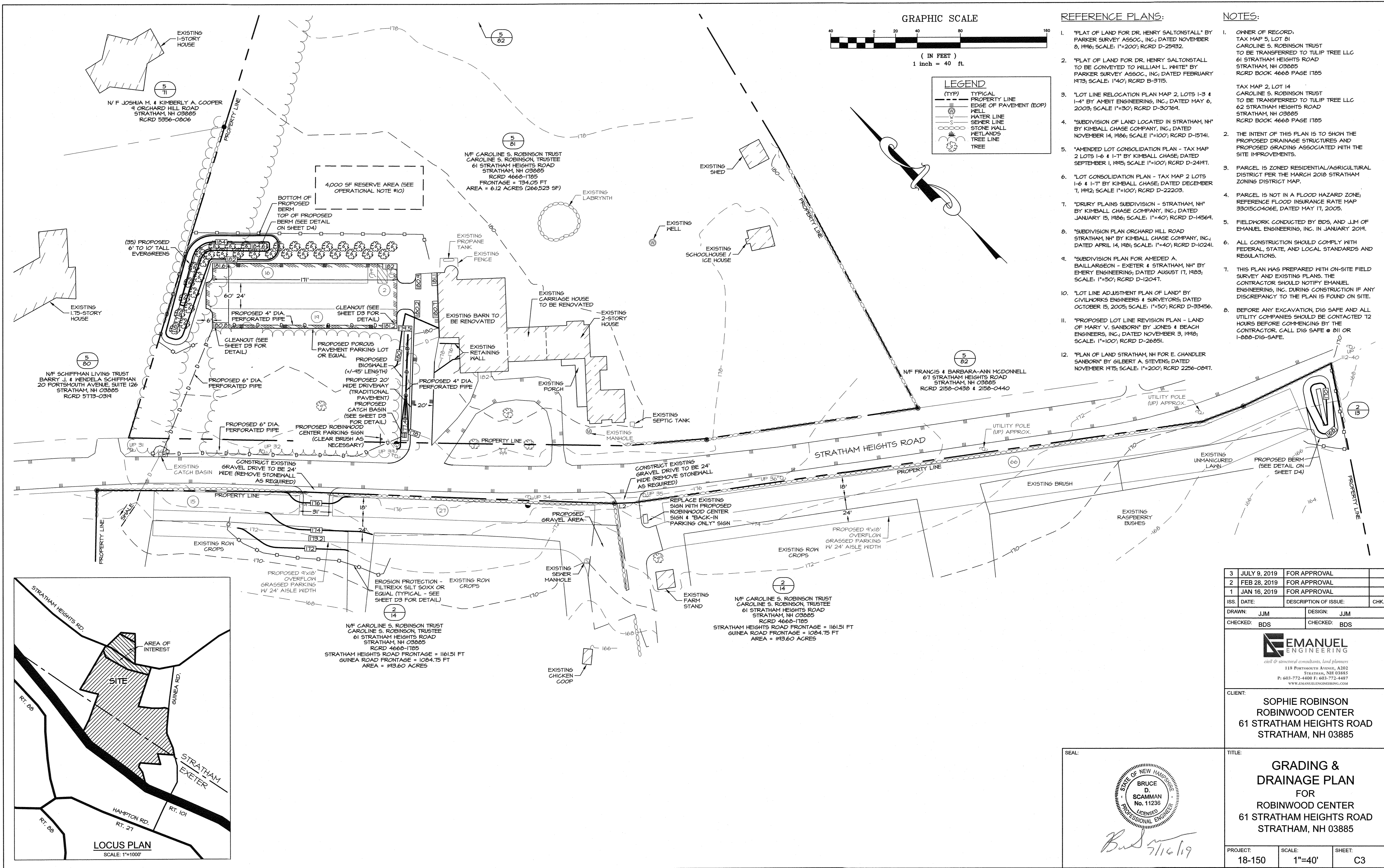
CHAIRPERSON _____ DATE _____

I CERTIFY THAT THIS SURVEY PLAT WAS PREPARED BY ME OR THOSE UNDER MY DIRECT SUPERVISION. THIS PLAN IS A RESULT OF FIELD SURVEY PERFORMED BY BDS AND JIM OF EEL IN JANUARY 2019. THE ERROR OF CLOSURE IS BETTER THAN 1/10,000. SURVEY PER NHLSA STANDARDS, CATEGORY I, CONDITION I. AREA CALCULATIONS ARE PER CARLSON CIVIL SUITE 2018 SOFTWARE.

[Signature] 7/16/19
LICENSED LAND SURVEYOR, DATE



3 JULY 9, 2019 FOR RECORDING	
2 FEB 28, 2019 FOR APPROVAL	
1 JAN 16, 2019 FOR APPROVAL	
ISS. DATE:	DESCRIPTION OF ISSUE: CHK.
DRAWN: JMM	DESIGN: JMM
CHECKED: BDS	CHECKED: BDS
EMANUEL ENGINEERING civil & structural consultants, land planners 118 FORTSMOUTH AVENUE, A202 STRATHAM, NH 03885 P: 603-772-4400 F: 603-772-4487 WWW.EMANUELENGINEERING.COM	
CLIENT: SOPHIE ROBINSON ROBINWOOD CENTER 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885	
TITLE: SITE PLAN FOR TULIP TREE LLC (OWNER) AND ROBINWOOD CENTER 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885	
PROJECT: 18-150	SCALE: 1"=40'
SHEET: C2B	



REFERENCE PLANS:

- "PLAT OF LAND FOR DR. HENRY SALTONSTALL" BY PARKER SURVEY ASSOC., INC.; DATED NOVEMBER 8, 1946; SCALE: 1"=200'; RCRD D-25432.
- "PLAT OF LAND FOR DR. HENRY SALTONSTALL TO BE CONVEYED TO WILLIAM L. WHITE" BY PARKER SURVEY ASSOC., INC.; DATED FEBRUARY 1973; SCALE: 1"=40'; RCRD B-3715.
- "LOT LINE RELOCATION PLAN MAP 2, LOTS 1-3 & 1-4" BY AMBIT ENGINEERING, INC.; DATED MAY 6, 2003; SCALE: 1"=30'; RCRD D-30764.
- "SUBDIVISION OF LAND LOCATED IN STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC.; DATED NOVEMBER 14, 1986; SCALE: 1"=100'; RCRD D-15741.
- "AMENDED LOT CONSOLIDATION PLAN - TAX MAP 2 LOTS 1-6 & 1-7" BY KIMBALL CHASE, DATED SEPTEMBER 1, 1995; SCALE: 1"=100'; RCRD D-24197.
- "LOT CONSOLIDATION PLAN - TAX MAP 2 LOTS 1-6 & 1-7" BY KIMBALL CHASE, DATED DECEMBER 7, 1992; SCALE: 1"=100'; RCRD D-22203.
- "DRURY PLAINS SUBDIVISION - STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC.; DATED JANUARY 15, 1986; SCALE: 1"=40'; RCRD D-14564.
- "SUBDIVISION PLAN ORCHARD HILL ROAD STRATHAM, NH" BY KIMBALL CHASE COMPANY, INC.; DATED APRIL 14, 1981; SCALE: 1"=40'; RCRD D-10241.
- "SUBDIVISION PLAN FOR AMEDED A. BAILLARGEON - EXETER & STRATHAM, NH" BY EMERY ENGINEERING, DATED AUGUST 17, 1983; SCALE: 1"=50'; RCRD D-12047.
- "LOT LINE ADJUSTMENT PLAN OF LAND" BY CIVILWORKS ENGINEERS & SURVEYORS, DATED OCTOBER 15, 2005; SCALE: 1"=50'; RCRD D-33456.
- "PROPOSED LOT LINE REVISION PLAN - LAND OF HARRY V. SANBORN" BY JONES & BEACH ENGINEERS, INC.; DATED NOVEMBER 3, 1998; SCALE: 1"=100'; RCRD D-26851.
- "PLAN OF LAND STRATHAM, NH FOR E. CHANDLER SANBORN" BY GILBERT A. STEVENS, DATED NOVEMBER 1975; SCALE: 1"=200'; RCRD 2256-0847.

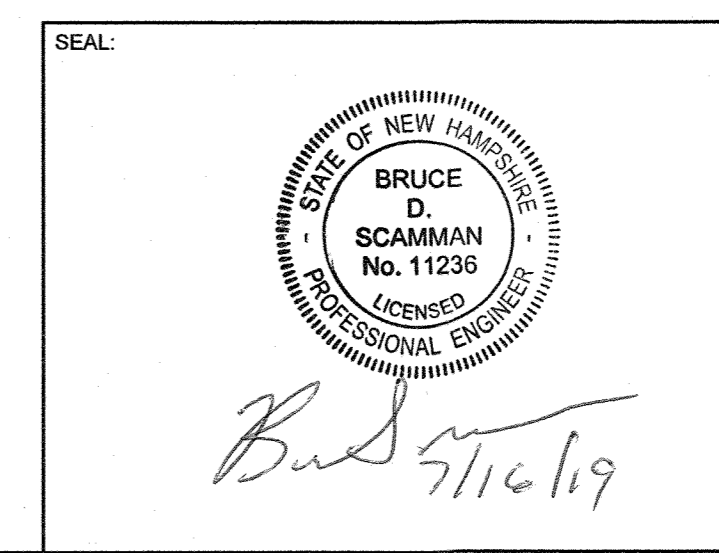
NOTES:

- OWNER OF RECORD: TAX MAP 5, LOT 81 CAROLINE S. ROBINSON TRUST TO BE TRANSFERRED TO TULIP TREE LLC 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885 RCRD BOOK 4668 PAGE 1785
- TAX MAP 2, LOT 14 CAROLINE S. ROBINSON TRUST TO BE TRANSFERRED TO TULIP TREE LLC 62 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885 RCRD BOOK 4668 PAGE 1785
- THE INTENT OF THIS PLAN IS TO SHOW THE PROPOSED DRAINAGE STRUCTURES AND PROPOSED GRADING ASSOCIATED WITH THE SITE IMPROVEMENTS.
- PARCEL IS ZONED RESIDENTIAL/AGRICULTURAL DISTRICT PER THE MARCH 2018 STRATHAM ZONING DISTRICT MAP.
- PARCEL IS NOT IN A FLOOD HAZARD ZONE; REFERENCE FLOOD INSURANCE RATE MAP 33015C0406E, DATED MAY 17, 2005.
- FIELDWORK CONDUCTED BY BDS, AND JIM OF EMANUEL ENGINEERING, INC. IN JANUARY 2019.
- ALL CONSTRUCTION SHOULD COMPLY WITH FEDERAL, STATE, AND LOCAL STANDARDS AND REGULATIONS.
- THIS PLAN WAS PREPARED WITH ON-SITE FIELD SURVEY AND EXISTING PLANS. THE CONTRACTOR SHOULD NOTIFY EMANUEL ENGINEERING, INC. DURING CONSTRUCTION IF ANY DISCREPANCY TO THE PLAN IS FOUND ON SITE.
- BEFORE ANY EXCAVATION, DIG SAFE AND ALL UTILITY COMPANIES SHOULD BE CONTACTED 72 HOURS BEFORE COMMENCING BY THE CONTRACTOR. CALL DIG SAFE @ 811 OR 1-888-DIG-SAFE.

3	JULY 9, 2019	FOR APPROVAL	
2	FEB 28, 2019	FOR APPROVAL	
1	JAN 16, 2019	FOR APPROVAL	
ISS. DATE:	DESCRIPTION OF ISSUE:		CHK.
DRAWN: JIM	DESIGN: JIM		
CHECKED: BDS	CHECKED: BDS		

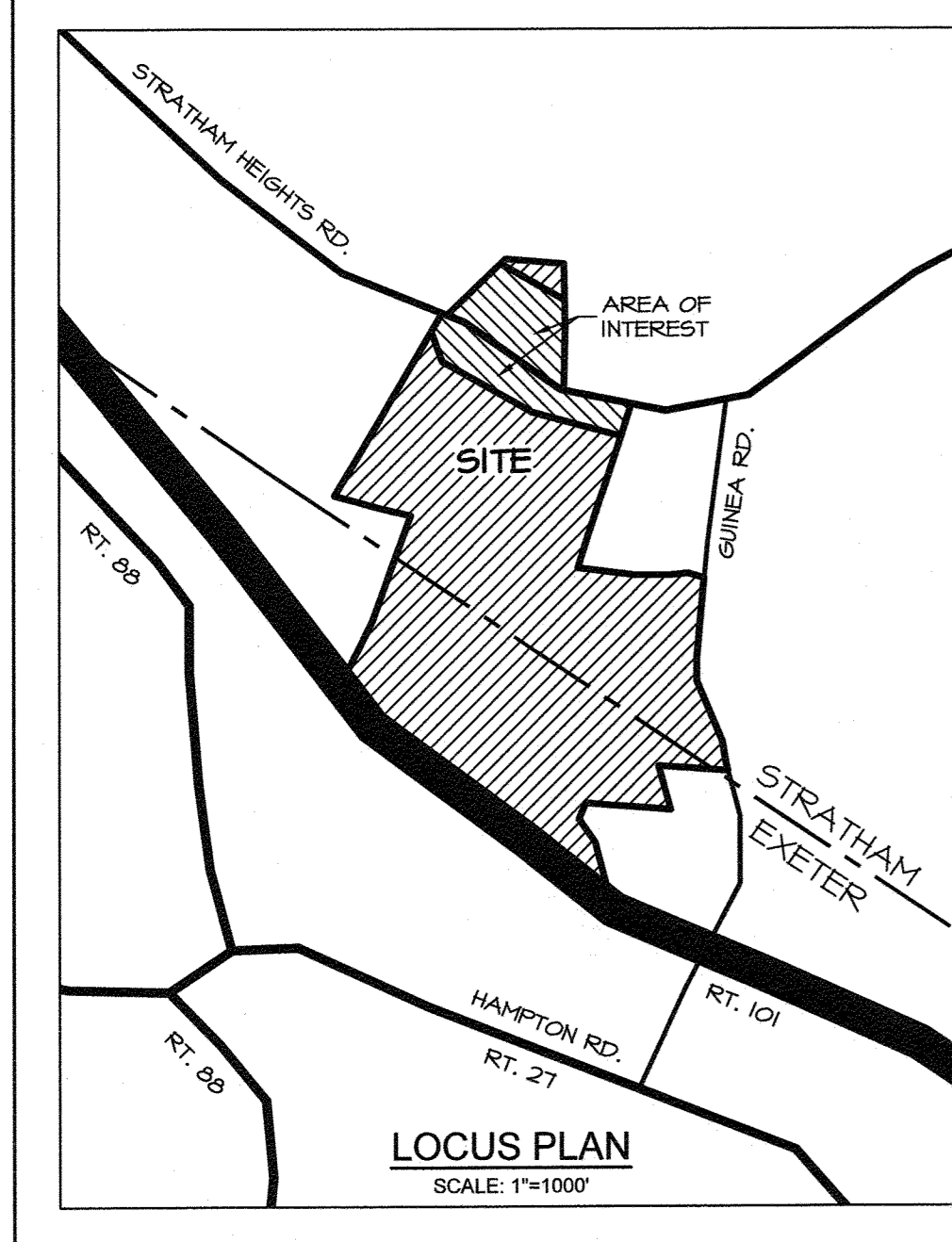


CLIENT:
**SOPHIE ROBINSON
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885**



TITLE:
**GRADING &
DRAINAGE PLAN
FOR
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885**

PROJECT:	SCALE:	SHEET:
18-150	1"=40'	C3



PROJECT: 18-150	SCALE: AS SHOWN	SHEET: D1

SECTION IV- POROUS ASPHALT PAVING (POROUS ASPHALT PAVEMENTS)

DR. ROBERT ROSEN OF WATERSTONE ENGINEERING, INC. (OR EQUAL) SHALL REVIEW ALL ASPECTS OF PREPARATION, INSTALLATION, AND TESTING OF THE POROUS PAVEMENT SECTIONS. PHONE: (603) 686-2488

PART 1

- 1.01 SCHEDULING
- A. SCHEDULE THE PAVING OPERATIONS SUCH THAT ALL PAVING NECESSARY TO PROVIDE SAFE AND ADEQUATE MAINTENANCE AND PROTECTION OF TRAFFIC OR FOR PROTECTION OF PREVIOUSLY LAID COURSES IS COMPLETED WITHIN THE WEATHER AND SEASONAL LIMITATIONS.
1. SUCH SCHEDULING SHALL INCLUDE EXPEDITING CONSTRUCTION OPERATIONS TO PERMIT PAVING BEFORE THE SEASONAL LIMITATIONS OR BY LIMITING THE LENGTH OF WORK TO THAT WHICH CAN BE COMPLETED BEFORE THE SEASONAL SHUTDOWN.
2. THE COST OF SCHEDULING AND SEQUENCING OF WORK TO CONFORM TO THE SEASONAL LIMITATIONS SHALL BE REFLECTED IN THE BID PRICES FOR THE RELATED CONTRACT ITEMS.

PART 2

- 2.01 ASPHALT CONCRETE
- A. BINDER COURSE - THE PAVEMENT BINDER COURSE SHALL BE CONSTRUCTED AS FOLLOWS:
1. TYPE AND TO THE WIDTHS AND DEPTHS AS SHOWN ON THE DRAWINGS.
1. THIS BINDER COURSE SHALL BE IN ACCORDANCE WITH NDOT SPECIFICATION FOR BITUMINOUS CONCRETE.
- A. PAVEMENT WEARING COURSE (SURFACE COURSE) - PAVEMENT WEARING COURSE SHALL BE CONSTRUCTED OF THE FOLLOWING TYPE AND TO THE WIDTH AND DEPTH AS SHOWN ON THE DRAWINGS.
1. THIS WEARING COURSE SHALL BE IN ACCORDANCE WITH NDOT SPECIFICATION FOR BITUMINOUS CONCRETE.
- A. PAINTED TRAFFIC MARKINGS - CONTRACTOR SHALL REPLACE ALL MARKINGS IN ACCORDANCE WITH LOCAL, COUNTY, OR STATE SPECIFICATIONS (DEPENDING ON JURISDICTION).

2.02 POROUS ASPHALT

- A. THIS IS A PERFORMANCE SPECIFICATION. ALTERNATIVES CAN BE SUBMITTED IF THE MIX DESIGN MEETS THE MINIMUM QC PERFORMANCE CRITERIA FOR GRADATION, ASPHALT CONCRETE (AC) CONTENT, PERCENT (%) VOID SPACE, % DRAIN DOWN, RETAINED TENSILE STRENGTH (TSR), AND CANTABRO WEAR TEST AND ACCEPTED IN WRITING BY THE ENGINEER.
- B. POLYMER MODIFIED PERFORMANCE GRADED ASPHALT BINDER AND MIX DESIGNS
1. POROUS ASPHALT COURSE: GRADATION, AC CONTENT, % VOID SPACE, % DRAIN DOWN, TSR, CANTABRO AS INDICATED IN TABLE 3. THE ASPHALT BINDER SHALL BE A TERMINAL BLENDED MIXTURE, FURNISHED WITH A STYRENE BUTADIENE STYRENE.
2. POROUS ASPHALT BINDER COURSE: GRADATION, AC CONTENT, % VOID SPACE, % DRAIN DOWN, TSR, CANTABRO AS INDICATED IN TABLE 3. THE ASPHALT BINDER SHALL BE A TERMINAL BLENDED PG76-28 MODIFIED WITH A STYRENE BUTADIENE STYRENE.
3. POROUS ASPHALT MIX DESIGNS: THE CONTRACTOR SHALL SIZE, UNIFORMLY GRADE, AND COMBINE THE AGGREGATE FRACTIONS IN PROPORTIONS THAT PROVIDE A MIXTURE MEETING THE REQUIREMENTS SPECIFIED.

PART 3

- 3.01 PREPARATION - RESET MANHOLE FRAMES
- A. PRIOR TO PLACING WEARING (TOP) COURSE, MAKE FINAL ADJUSTMENTS TO MANHOLE FRAMES, CATCH BASIN FRAMES, VALVE BOXES AND ANY OTHER UTILITY STRUCTURES LOCATED IN THE PAVEMENT IN RELATION TO FINISHED GRADE.
1. MANHOLE FRAMES, VALVE BOXES, ETC., TO SET 1/2 INCH BELOW FINISHED GRADE AND PARALLEL TO FINISHED CROWN.
2. CATCH BASIN FRAMES TO SET 1 INCH BELOW FINISHED GRADE AND PARALLEL TO FINISHED CROWN.
- a. BEVEL SLOPE OF WEARING COURSE (FOR 6-INCH WIDTH) AROUND CATCH BASIN FRAME.

3.02 POROUS ASPHALT BINDER COURSE INSTALLATION

- A. TEST STRIP (OPTIONAL)
1. AN OPTIONAL TEST STRIP SHALL BE CONDUCTED TO DETERMINE OPTIMAL COMPACTION PROCEDURES FOR THE BINDER COURSE AT A THICKNESS AS INDICATED IN THE DRAWINGS. THE TEST STRIP WILL BE CONSTRUCTED IN A PORTION OF THE SITE TO ESTABLISH AND ENSURE THE PROPER MIX DESIGN, PRODUCTION AND PLACEMENT.
2. THE TEST STRIP SHALL BE OVERSEEN BY THE ENGINEER.
3. TWO MIX SAMPLES SHALL BE COLLECTED AT THE ASPHALT PLANT BY A 3RD PARTY QC TECHNICIAN DURING BINDER COURSE PRODUCTION FROM EACH TEST STRIP FOR ASPHALT CONTENT, AND GRADATION.
4. FIELD TESTING OF INFILTRATION CAPACITY SHALL BE PERFORMED ON THE TEST STRIP FOR INFILTRATION BY THE ENGINEER.
5. TWO CORES SHALL BE COLLECTED FROM EACH TEST STRIP AND EVALUATED FOR COMPACTION, DENSITY, AND POROSITY.
6. THESE CRITERIA ONCE ESTABLISHED WILL BE APPLIED TO ALL POROUS ASPHALT INSTALLATIONS.
- A. CONDITIONING OF EXISTING SURFACE
1. THE CONTRACTOR SHALL THOROUGHLY CLEAN THE SURFACE UPON WHICH THE BINDER COURSE IS TO BE PLACED OF ALL OBJECTIONABLE MATERIAL.
- A. PREPARATION OF AGGREGATES
1. THE CONTRACTOR SHALL DRY AND HEAT THE AGGREGATES FOR THE BINDER COURSE TO THE REQUIRED TEMPERATURE.
- A. MIXING
1. THE CONTRACTOR SHALL COMBINE THE DRIED AGGREGATE IN THE MIXER IN THE AMOUNT OF EACH FRACTION OF AGGREGATE REQUIRED TO MEET THE SPECIFICATIONS. ONCE MIXED THE BINDER COURSE SHALL BE PLACED AS SOON AS POSSIBLE.

E. SPREADING AND FINISHING

1. ON AREAS WHERE SURFACE IRREGULARITIES OR UNAVOIDABLE OBSTACLES MAKE THE USE OF MECHANICAL SPREADING AND FINISHING IMPRACTICABLE, THE CONTRACTOR SHALL SPREAD AND RAKE THE BINDER COURSE WITH HAND TOOLS TO PROVIDE THE REQUIRED COMPACTED THICKNESS.
2. SOLVENT BASED AGENTS DEVELOPED TO STRIP ASPHALTS FROM AGGREGATES WILL NOT BE ALLOWED AS A RELEASE AGENT.
3. JOINTS SHALL BE FULLY COATED WITH PG68 76-28 JUST PRIOR TO THE PLACEMENT OF THE ADJOINING COURSE. AREAS THAT BECOME CONTAMINATED OR STRIPPED OF ASPHALT COATING WILL BE RETREATED WITH ASPHALT PRIOR TO PLACING THE ADJOINING COARSE.
- A. COMPACTION
1. THE ACTUAL METHODS AND EQUIPMENT USED TO COMPACT THE BINDER COURSE WILL BE DETERMINED DURING THE PLACEMENT AND COMPACTION OF THE TEST STRIP AND AS TABLE 2.
2. IMMEDIATELY AFTER THE ASPHALT TREATED PERMEABLE BASE HAS BEEN SPREAD, ASPHALT SURFACING MATERIALS, AND IRREGULARITIES ADJUSTED, THE CONTRACTOR SHALL THOROUGHLY AND UNIFORMLY COMPACT THE BINDER COURSE BY ROLLING.
3. THE BINDER COURSE SHALL BE COMPACTED BY A MAXIMUM OF THREE COMPLETE PASSES OF A STEEL ROLLER HAVING A MINIMUM HEIGHT OF 12 TONS OPERATED IN STATIC MODE, OR 10 TONS IF EQUIPPED WITH OSCILLATING COMPACTION AND OPERATED IN LOW FREQUENCY, LOW AMPLITUDE MODE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. PNEUMATIC ROLLERS WILL NOT BE USED TO COMPACT THE BINDER COURSE.
4. THE CONTRACTOR SHALL ROLL THE SURFACE WHEN THE MIXTURE IS IN THE PROPER CONDITION AND WHEN THE ROLLING DOES NOT CAUSE UNDUE DISPLACEMENT, CRACKING, OR SHOVING. THE CONTRACTOR SHALL PREVENT ADHESION OF THE BINDER COURSE TO THE ROLLERS BY APPLYING COMPACTORS WITHOUT THE USE OF FUEL OIL OR OTHER PETROLEUM, OR SOLVENT BASED RELEASE AGENTS. SOLVENTS DESIGNED TO STRIP ASPHALT FROM THE ENGINEER'S AGGREGATES WILL NOT BE PERMITTED AS RELEASE AGENTS ON EQUIPMENT, TOOLS OR BINDER COURSE SURFACES.
5. THE CONTRACTOR SHALL IMMEDIATELY CORRECT ANY DISPLACEMENT OCCURRING AS A RESULT OF THE REVERSING OF THE DIRECTION OF A ROLLER OR FROM OTHER AGGREGATE INFILTRATION TO THE SATISFACTION OF THE ENGINEER.
6. ANY OPERATION THAT RESULTS IN BREAKDOWN OF THE AGGREGATE SHALL BE DISCONTINUED.
- A. TRAFFIC
1. AFTER A 24 HOUR CURING PERIOD OF THE BINDER COURSE, LIMITED TRAFFIC MAY BE ROUTED OVER THE BINDER COURSE SURFACE, UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER. CONSTRUCTION EQUIPMENT, AND TRAFFIC SHALL BE PROHIBITED FROM TRAVELING OVER THE BINDER COURSE SURFACE UNTIL THE ENTIRE PAVEMENT STRUCTURE IS IN PLACE.
2. DAMAGE TO THE BINDER COURSE LAYER CAUSED BY CONSTRUCTION EQUIPMENT OR TRAFFIC SHALL BE REMEDIED BY COMPLETE REMOVAL REPLACEMENT OF THE DAMAGED AREA TO THE LIMITS DETERMINED BY THE ENGINEER. THERE WILL BE NO ADDITIONAL PAYMENT FOR REPAIRS, OR ASSOCIATED WORK.

3.03 PLACEMENT OF POROUS ASPHALT BINDER COURSE

- A. INSTALL THE BINDER COURSE COURSE AT A THICKNESS AS INDICATED IN THE DRAWINGS.
- B. INSTALL FRAME, GRATES, AND LANDSCAPING. SPECIAL CARE IS TO BE TAKEN TO PROTECT FRESH BINDER COURSE.
- C. ALL TRUCKS (INCLUDING CONCRETE TRUCKS) WILL BE STOPPED PRIOR TO ENTERING THE SITE AND INSTRUCTED AS TO SPECIAL CONCERNS FOR PAVEMENT DURABILITY.
- D. A WASHOUT AREA FOR ALL CONCRETE TRUCKS SHALL BE DESIGNATED OUTSIDE OF POROUS PAVEMENT AREA ON THE CONTRACTOR'S EROSION AND SEDIMENT CONTROL PLAN. POROUS PAVEMENT SURFACE SHALL BE PROTECTED ON HOT DAYS DURING THE PAVEMENT CURE PERIOD (2-3 DAYS). SURFACE TEMPERATURES CAN QUICKLY REACH OVER 145°F IN DIRECT SUN.
- F. A TEMPERATURE GUN SHALL BE AVAILABLE ON SITE TO ASSESS PAVEMENT SURFACE TEMPERATURES. PAVEMENT TEMPERATURES GREATER THAN 100°F SHOULD BE OBSERVED CAREFULLY FOR PAVEMENT DURABILITY. AS NEEDED, COOLING OF PAVEMENT SURFACE BY APPLICATION OF WATER FROM A WATER TRUCK SHOULD OCCUR WHEN HEAVY VEHICULAR TRAFFIC IS EXPECTED SUCH AS CONCRETE TRUCKS FOR DRY HELL FRAME AND GRATE INSTALLATION. IN THE EVENT THIS IS INEFFECTIVE FOR COOLING AND PAVEMENT DEFORMATION IS STILL OBSERVED, THE USE OF 3/4" PLYWOOD UNDER LARGE VEHICLE WHEELS MAY BE REQUIRED.
- G. TRUCKS AND OTHER CONSTRUCTION TRAFFIC WILL NOT BE ALLOWED TO ACCESS THE SITE WHILE THE PAVEMENT IS EXCESSIVELY HOT >130°F.
- H. NO STOCKPILING OF MATERIALS (E.G. SOIL, STONE, LANDSCAPING MATERIALS) WILL BE ALLOWED ON POROUS PAVEMENTS.
- I. MATERIALS EXCAVATED FOR FINISH WORKS SHALL BE PLACED OUTSIDE OF POROUS PAVEMENT AREAS.
- J. VACUUMING THROUGHOUT CONSTRUCTION MAY BE NECESSARY FOR SURROUNDING PAVED AREAS TO PREVENT RUN-ON OR TRACKING ONTO POROUS PAVEMENTS. FREQUENCY SHALL BE ADJUSTED AS NEEDED.

3.04 INSPECTION, CORRECTIVE ACTION, REMOVAL

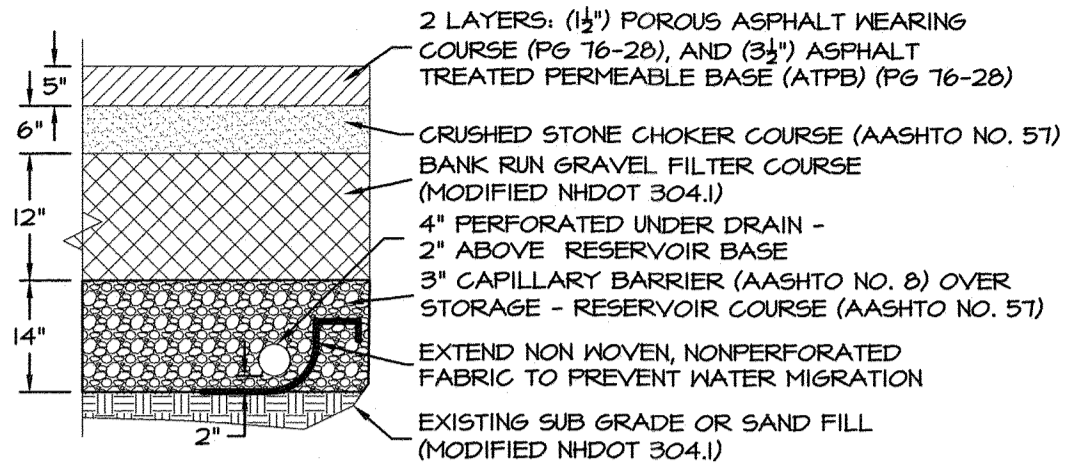
- A. PRIOR TO INSTALLATION OF THE POROUS ASPHALT WEARING COURSE, THE BINDER COURSE WILL BE INSPECTED FOR DAMAGE AND REDUCED INFILTRATION CAPACITY.
- B. BINDER COURSE DEFECTS WILL BE CORRECTED BY VACUUM AND PRESSURE WASHING TO THE SATISFACTION OF THE ENGINEER.
- C. BINDER COURSE AREAS WILL BE REMOVED AND REPLACED IN AREAS WHERE STRUCTURAL DAMAGE OR INFILTRATION CAPACITY IS SUBSTANTIALLY CONFORMED AT THE DISCRETION OF THE ENGINEER.
- D. THE CHOKER COURSE SHALL BE INSPECTED FOR VOIDS AND FINES PRIOR TO REPLACEMENT OF THE BINDER COURSE. IF VOID SPACE IS OBSERVED, STONE WILL BE ADDED AND/OR REPLACED FOR CORRECTIVE ACTION.
- E. THERE WILL BE NO ADDITIONAL PAYMENT FOR BINDER COURSE REPAIRS, CONTAMINANT REPLACEMENT, OR ASSOCIATED WORK.

3.05 POROUS ASPHALT WEARING COURSE

- A. GENERAL
1. VERIFY BINDER COURSE CONDITION AND PREPARATION FOLLOWING CONSTRUCTION PRIOR TO PAVING POROUS ASPHALT WEARING COURSE AS DESCRIBED IN SECTION 3.01.
2. THE ENGINEER SHALL BE NOTIFIED AND INSPECTED 12 HOURS PRIOR TO THEIR DISCRETION PRIOR TO PAVING THE POROUS ASPHALT WEARING COURSE.
3. TEMPORARY CONSTRUCTION FENCINGS WILL BE USED TO CLOSE POROUS PAVEMENT AREAS TO CONSTRUCTION TRAFFIC AFTER PAVING DURING PROJECT COMPLETION.
- B. TEST STRIP
1. A TEST STRIP SHALL BE CONDUCTED TO DETERMINE OPTIMAL COMPACTION PROCEDURES OF THE POROUS ASPHALT AT A THICKNESS AS INDICATED IN THE DRAWINGS. THE TEST STRIP WILL BE CONSTRUCTED IN A PORTION OF THE SITE TO ESTABLISH AND ENSURE THE PROPER MIX DESIGN, PRODUCTION AND PLACEMENT.
2. THE TEST STRIP SHALL BE OVERSEEN BY THE ENGINEER.
3. TWO MIX SAMPLES SHALL BE COLLECTED AT THE ASPHALT PLANT BY A 3RD PARTY QC TECHNICIAN DURING PRODUCTION FROM EACH TEST STRIP FOR ASPHALT CONTENT, GRADATION, AND CANTABRO WEAR.
4. FIELD TESTING OF INFILTRATION CAPACITY SHALL BE PERFORMED ON THE TEST STRIP FOR INFILTRATION BY THE ENGINEER.
5. TWO CORES SHALL BE COLLECTED FROM EACH TEST STRIP AND EVALUATED FOR COMPACTION, DENSITY, AND POROSITY.
6. THESE CRITERIA ONCE ESTABLISHED WILL BE APPLIED TO ALL POROUS ASPHALT INSTALLATIONS.
- C. ROLLERS
1. ROLLERS OR OSCILLATING VIBRATORY ROLLERS, RANGING FROM 8-12 TONS, SHALL BE USED FOR COMPACTION, AND 1-2 TONS ROLLER FOR FINISHING. THE NUMBER, MASS (WEIGHT), AND TYPE OF ROLLERS FURNISHED SHALL BE SUFFICIENT TO OBTAIN THE REQUIRED COMPACTION WHILE THE MIXTURE IS IN A WORKABLE CONDITION. GENERALLY, ONE BREAKDOWN ROLLER WILL BE NEEDED FOR EACH PAYER USED IN THE SPREADING OPERATION.
2. ADDITIONAL ROLLING MAY BE EXCESSIVE, CAUSING A BREAK IN THE BOND OF ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
3. TO PREVENT ADHESION OF THE MIXTURE TO THE ROLLERS, ROLLERS SHALL BE KEPT moist with WATER OR WATER MIXED WITH VERY SMALL QUANTITIES OF DETERGENT OR OTHER APPROVED MATERIAL. EXCESS LIQUID WILL BE KEPT OFF THE MIX.
4. OTHER COMBINATIONS OF ROLLERS AND/OR METHODS OF COMPACTING MAY BE USED IF APPROVED IN WRITING BY THE ENGINEER.
5. PROVIDED THE ROLLERS AND THEIR OPERATIONS ARE MET, THE SPEED OF THE ROLLER SHALL BE SLOW AND UNIFORM TO AVOID DISPLACEMENT OF THE MIXTURE, AND THE ROLLER SHOULD BE KEPT IN A TRENCH OPERATION AS PRACTICAL. ROLLING SHALL CONTINUE UNTIL ALL ROLLER MARKS AND RIDGES HAVE BEEN ELIMINATED.
6. ROLLERS WILL NOT BE STOPPED OR PARKED ON THE FRESHLY PLACED MAT. THE SPEED OF THE ROLLER SHALL BE SLOW AND UNIFORM TO AVOID DISPLACEMENT OF THE MIXTURE, AND THE ROLLER SHOULD BE KEPT IN A TRENCH OPERATION AS PRACTICAL. ROLLING SHALL CONTINUE UNTIL ALL ROLLER MARKS AND RIDGES HAVE BEEN ELIMINATED.
7. ROLLERS WILL NOT BE STOPPED OR PARKED ON THE FRESHLY PLACED MAT.
- D. CONDITIONING OF EXISTING SURFACE
1. CONTACT SURFACES SUCH AS CURBING, GUTTERS, AND MANHOLES SHALL BE PAINTED WITH A THIN, UNIFORM COAT OF TYPE RS-1 EMULSIFIED ASPHALT IMMEDIATELY BEFORE THE ASPHALT MIXTURE IS PLACED AGAINST THEM.
- E. TEMPERATURE REQUIREMENTS
1. THE TEMPERATURE OF THE ASPHALT MIXTURE, AT THE TIME OF DISCHARGE FROM THE HAUL VEHICLE AND AT THE PAYER, SHALL BE BETWEEN 135-163°C (275 TO 325°F), WITHIN 6°C (10 °F) OF THE COMPACTION TEMPERATURE FOR THE APPROVED MIX DESIGN.
2. THE TEMPERATURE OF THE ASPHALT MIXTURE, AT THE TIME OF DISCHARGE FROM THE HAUL VEHICLE AND AT THE PAYER, SHALL BE BETWEEN 135-163°C (275 TO 325°F), WITHIN 6°C (10 °F) OF THE COMPACTION TEMPERATURE FOR THE APPROVED MIX DESIGN.
3. BREAKDOWN ROLLING SHALL OCCUR WHEN THE MIX TEMPERATURE IS BETWEEN 135-163°C (275 TO 325°F).
4. INTERMEDIATE ROLLING SHALL OCCUR WHEN THE MIX TEMPERATURE IS BETWEEN 43-135°C (200 TO 275°F).
5. FINISH ROLLING SHALL OCCUR WHEN THE MIX TEMPERATURE IS BETWEEN 66-43°C (150 TO 200°F).

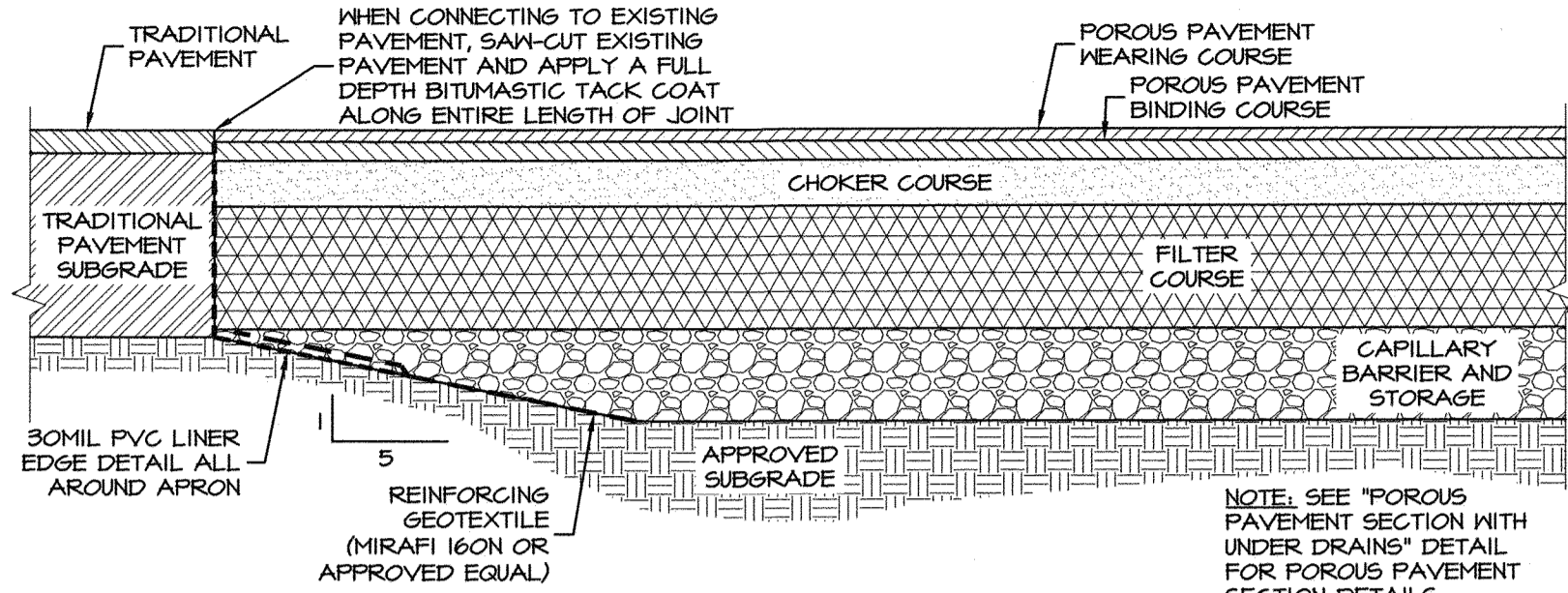
F. SPREADING AND FINISHING

1. THE PAVEMENT WEARING COURSE SHALL BE PLACED IN ONE APPLICATION TO A THICKNESS AS INDICATED ON THE DRAWINGS.
2. THE CONTRACTOR SHALL PROTECT ALL EXPOSED SURFACES THAT ARE NOT TO BE TREATED FROM DAMAGE DURING ALL PHASES OF THE PAVEMENT OPERATION.
3. NO TRAFFIC SHALL BE ALLOWED ON THE PAVEMENT UNTIL THE MATERIAL HAS BEEN THOROUGHLY COMPACTED AND HAS BEEN PERMITTED TO COOL TO BELOW 38 °C (100 °F). THE ENGINEER RESERVES THE RIGHT TO REQUEST THE CONTRACTOR TO ADJACENT TO THE PAVEMENT, SUCH AS GUARDRAIL, CLEANUP, AND TURF ESTABLISHMENT, IS COMPLETED PRIOR TO PLACING THE WEARING COURSE WHEN THIS WORK COULD CAUSE DAMAGE TO THE PAVEMENT.
6. COMPACTION
1. PLACING MIX IN AN APPROPRIATE AMBIENT TEMPERATURE AND ON A SURFACE SUFFICIENTLY WARM TO MINIMIZE THE RISK OF EXCESSIVE COOLING BEFORE COMPACTION. SURFACE TEMPERATURE IMPORTANCE. HOLDING THE AGGREGATE PARTICLES IN PLACE IS SOLELY THE FUNCTION OF THE FILM OF ASPHALT. THE ASPHALT SHOULD BE PLACED AT A TEMPERATURE PROPERLY IF THE MIX IS TOO COOL WHEN ROLLED.
2. IT IS IMPORTANT TO KEEP THE TIME BETWEEN THE PLACEMENT AND COMPACTION OF THE BINDER COURSE AND BINDER COURSE PLACEMENTS SHALL BE MINIMAL AND KEEP THE FIRST LAYER CLEAR FROM DUST AND MOISTURE AND MINIMIZE TRAFFIC ON THE PAVEMENT.
3. A THIN COURSE COMPRESSES VERY LITTLE UNDER THE ROLLER AND, AS IT COOLS QUICKLY, IT MUST BE ROLLED AS SOON AS POSSIBLE.
4. A HIGH DEGREE OF DENSIIFICATION IS NOT THE GOAL WITH THIS TYPE OF MIX; THE AIM IS FIRM SEATING AND CONTACT OF THE AGGREGATE PARTICLES.
5. WHEN OVERTAKEN BY SUDDEN STORMS, THE ENGINEER MAY PERMIT WORK TO CONTINUE UP TO THE AMOUNT WHICH MAY BE IN TRANSIT FROM THE PLANT AT THE TIME, PROVIDED THE MIXTURE IS WITHIN TEMPERATURE LIMITS SPECIFIED.
6. IMMEDIATELY AFTER THE ASPHALT MIXTURE HAS BEEN SPREAD, SURFACE IRREGULARITIES ADJUSTED, IT SHALL BE THOROUGHLY AND UNIFORMLY COMPACTED BY ROLLING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR 22% IN PLACE VOID CONTENT (CORELOCK). GREAT CARE SHALL BE TAKEN TO AVOID OVER-COMPACTION.
7. IF THE ENGINEER DETERMINES THAT UNSATISFACTORY COMPACTION OR SURFACE DISTORTION IS BEING OBTAINED OR DAMAGE TO HIGHWAY COMPONENTS AND/OR ADJACENT AREAS, THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
8. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
9. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
10. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
11. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
12. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
13. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
14. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
15. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
16. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
17. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
18. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
19. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
20. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
21. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
22. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
23. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
24. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
25. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
26. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
27. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
28. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
29. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
30. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
31. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
32. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
33. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
34. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
35. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
36. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
37. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
38. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
39. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
40. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
41. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
42. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
43. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
44. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
45. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
46. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
47. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
48. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
49. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
50. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
51. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
52. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
53. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
54. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
55. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
56. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
57. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
58. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
59. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
60. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
61. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
62. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
63. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
64. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
65. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
66. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
67. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
68. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
69. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
70. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
71. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
72. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
73. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
74. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
75. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
76. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
77. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
78. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
79. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
80. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
81. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
82. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
83. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
84. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
85. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
86. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
87. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
88. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
89. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
90. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
91. THE MINIMUM DEPTH OF SUBBASE SHALL BE AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL IMMEDIATELY CEASE USING THIS EQUIPMENT AND PROCEED WITH THE WORK IN ACCORDANCE WITH THE FIFTH PARAGRAPH OF THIS SUBSECTION.
92. ALONG FORMS, CURBS, HEADERS, WALLS, AND OTHER PLACES NOT ACCESSIBLE TO THE ROLLERS, THE MIXTURE SHALL BE THOROUGHLY COMPACTED WITH HOT OR LIGHTLY OILED HAND TAMPERS, SMOOTHING IRONS OR WITH MECHANICAL TAMPERS. ON ASPHALT BETWEEN EXISTING PARTICLES, PARTICULARLY AFTER THE MIX HAS COOLED.
- 9



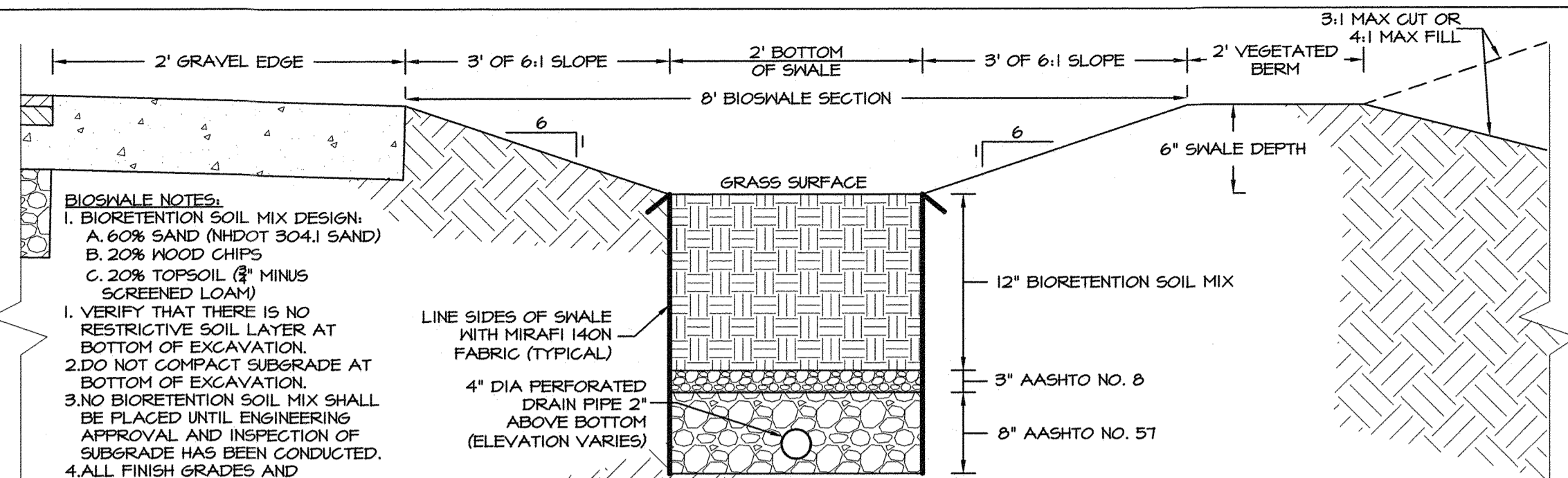
NOTE:
1. SEE SHEETS D1 & D2 FOR ADDITIONAL NOTES ON POROUS PAVEMENT, AND TABLE 1 SPECIFICALLY FOR MODIFIED NHDOT 304.1 GRADATION.

POROUS PAVEMENT SECTION WITH UNDER DRAINS
N.T.S.



NOTE: SEE "POROUS PAVEMENT SECTION WITH UNDER DRAINS" DETAIL FOR POROUS PAVEMENT SECTION DETAILS

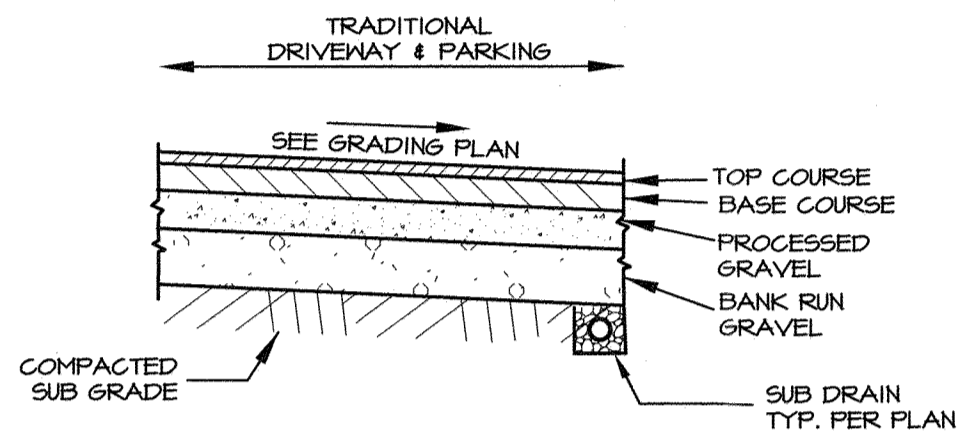
POROUS PAVEMENT TRANSITION DETAIL
N.T.S.



BIOSWALE NOTES:
1. BIORETENTION SOIL MIX DESIGN:
A. 60% SAND (NHDOT 304.1 SAND)
B. 20% WOOD CHIPS
C. 20% TOPSOIL (3 inch MINUS SCREENED LOAM)
2. DO NOT COMPACT SUBGRADE AT BOTTOM OF EXCAVATION.
3. NO BIORETENTION SOIL MIX SHALL BE PLACED UNTIL ENGINEERING APPROVAL AND INSPECTION OF SUBGRADE HAS BEEN CONDUCTED.
4. ALL FINISH GRADES AND SURFACES TO BE STABILIZED BY VEGETATION UPON COMPLETION UNLESS NOTED OTHERWISE.
5. SEE LAYOUT PLAN FOR STRUCTURE ELEVATION AND INVERTS, PIPE TYPE, LENGTH, AND SLOPE, AND BIORETENTION AREA DIMENSIONS

NOTES:
1. 4" PERFORATED PIPE IS TO BE SDR-35 WITH 1/4" DIAMETER PERFORATIONS, 3 HOLES MINIMUM PER FOOT OF LENGTH

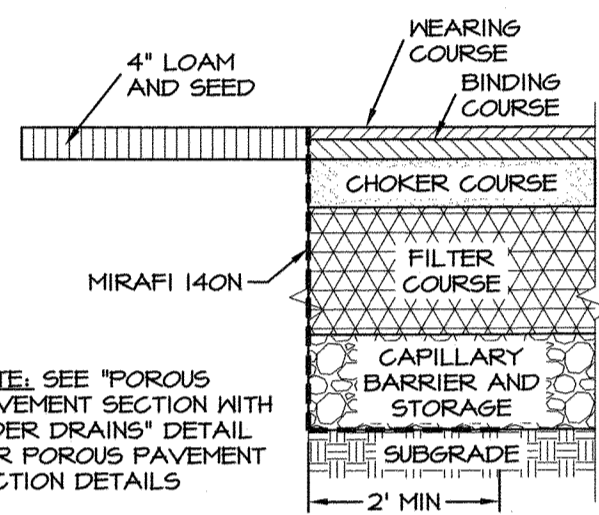
BIOSWALE DETAIL
NOT TO SCALE



PAVEMENT COMPONENTS:
MATERIALS AND PLACEMENT METHODS SHOULD MEET CURRENT NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION (NHDOT) REQUIREMENTS.

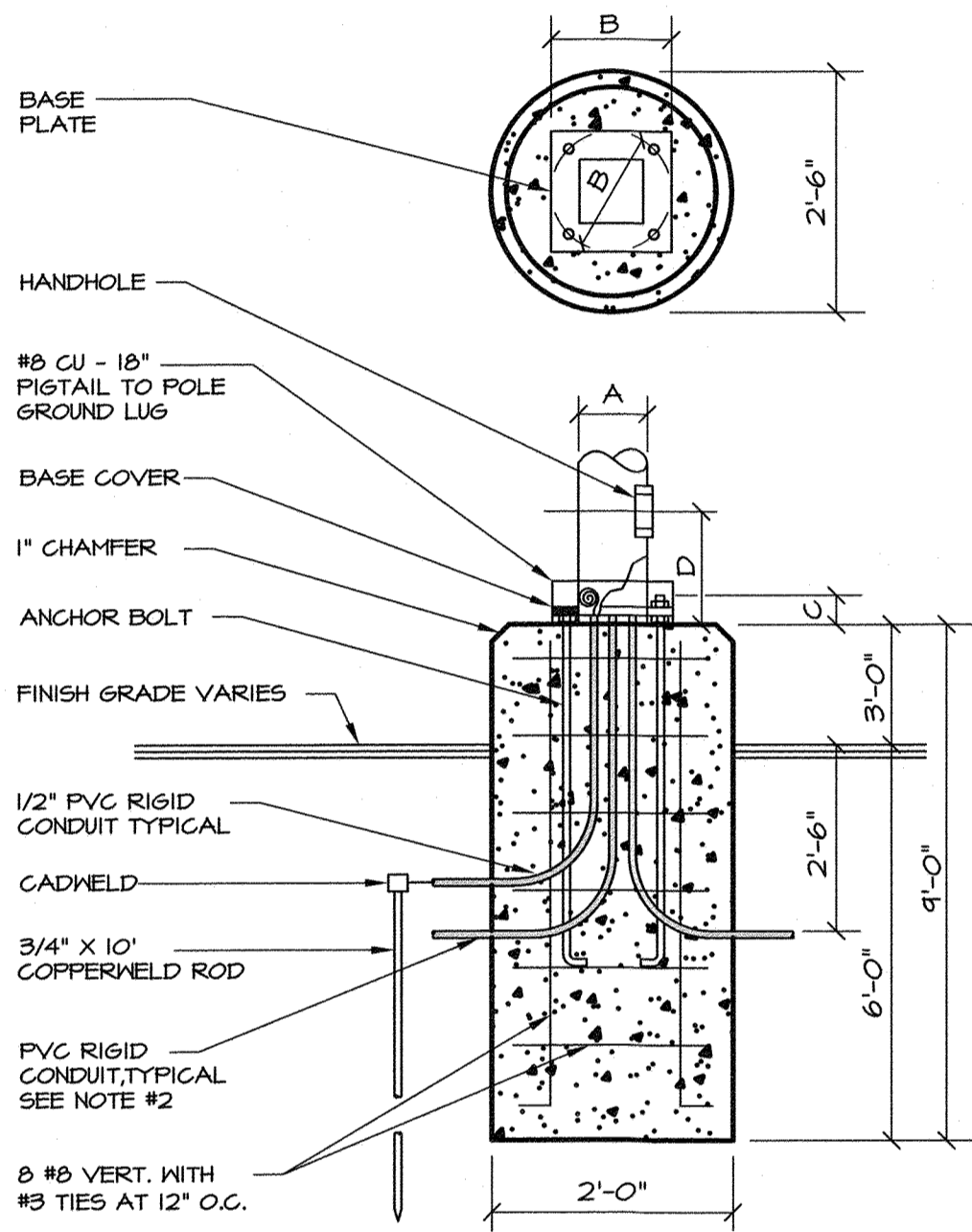
DRIVEWAY:
2" ASPHALT WEARING SURFACE, NHDOT 1/2" NOMINAL MAXIMUM AGGREGATE SIZE
3" ASPHALT BASE, NHDOT 3/4" NOMINAL MAXIMUM AGGREGATE SIZE
6" CRUSHED GRAVEL BASE, NHDOT TYPE 304.3
12" GRAVEL SUB BASE, NHDOT TYPE 304.2

TRADITIONAL DRIVEWAY & PARKING LOT SECTION
N.T.S.



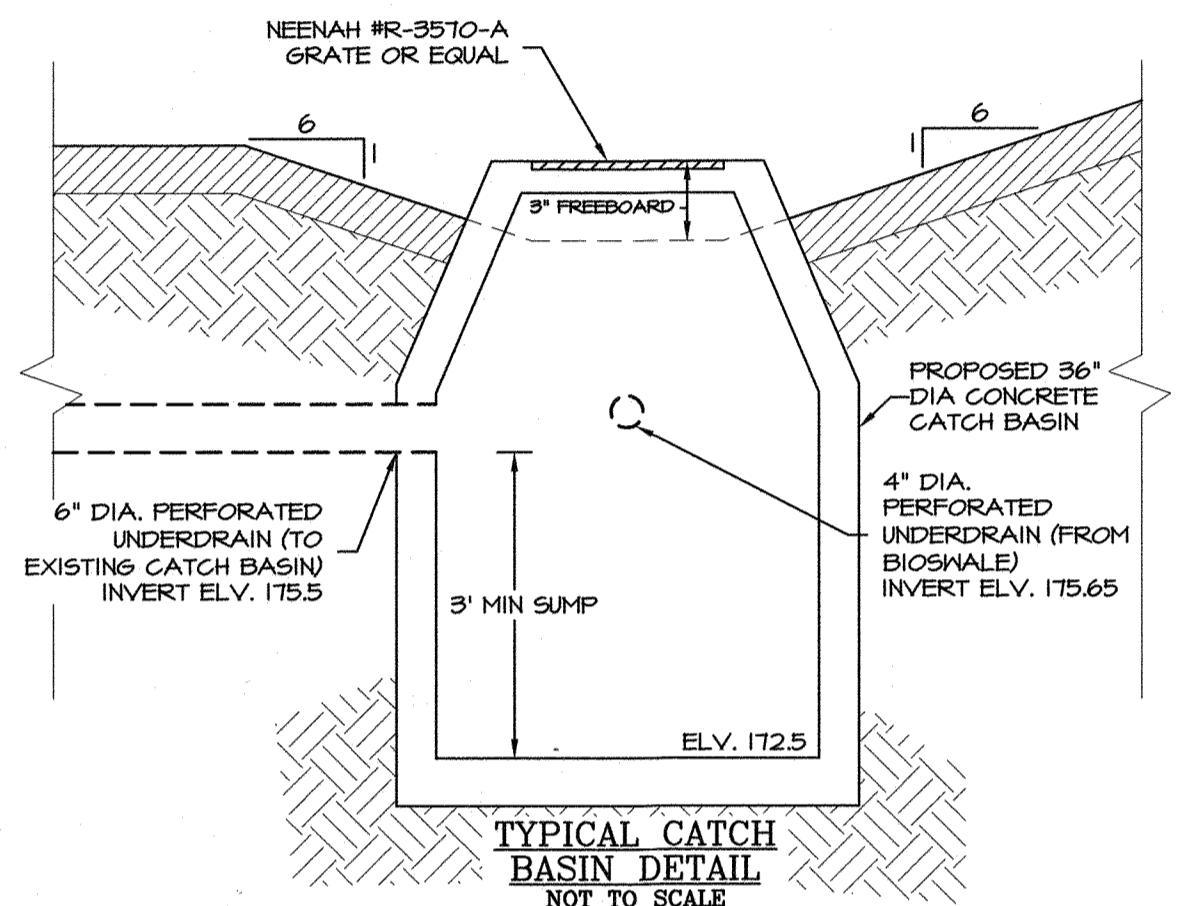
NOTE: SEE "POROUS PAVEMENT SECTION WITH UNDER DRAINS" DETAIL FOR POROUS PAVEMENT SECTION DETAILS

POROUS PAVEMENT EDGE DETAIL
N.T.S.



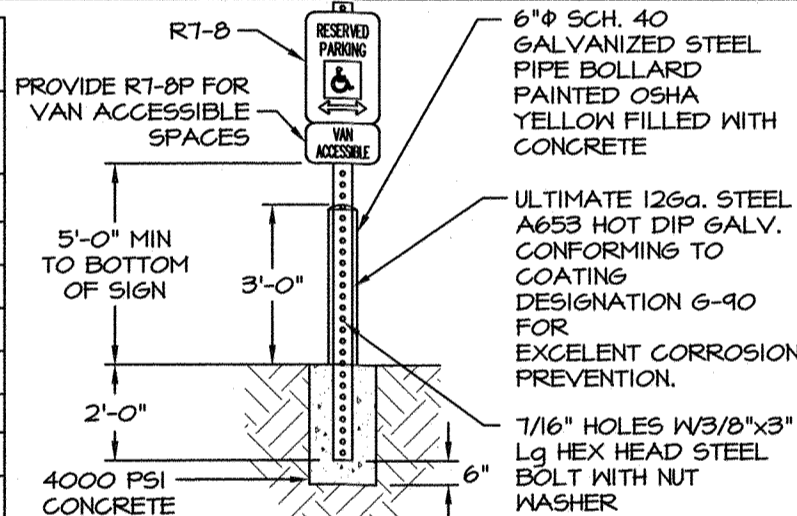
NOTES:
1. REFER TO MANUFACTURER FOR A, B, C AND D DIMENSIONS.
2. FIELD COORDINATE QUANTITY AND SIZE OF CONDUITS AT EACH LOCATION.
3. CONCRETE BASE BY GENERAL CONTRACTOR.

LIGHT POLE CONCRETE BASE DETAIL
N.T.S.



TYPICAL CATCH BASIN DETAIL
NOT TO SCALE

PARKING & PASSENGER LOADING ZONES			
TOTAL # PARKING SPACES	STAND.	VAN	TOTAL
1 - 25	0	1	1
26 - 50	1	1	2
51 - 75	2	1	3
76 - 100	3	1	4
101 - 150	4	1	5
151 - 200	5	1	6
201 - 300	6	1	7
301 - 400	7	1	8
401 - 500	8	2	10
501 - 550	9	2	11
551 - 600	10	2	12
601 - 650	11	2	13



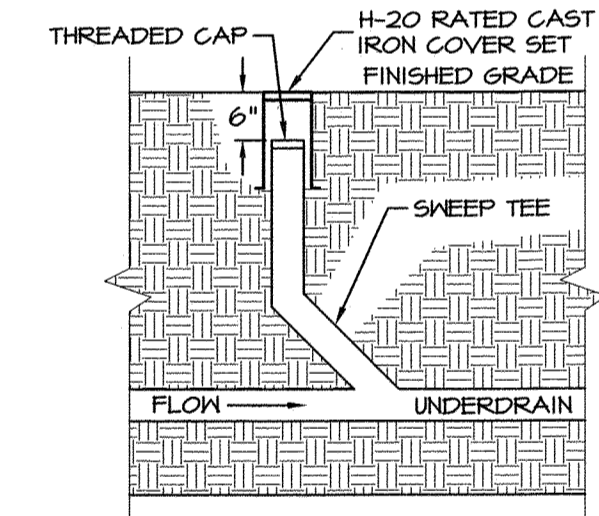
SIGN POST WITH ACCESSIBILITY SYMBOL & "VAN ACCESSIBLE" SIGN MOUNTED BELOW SIGN POST WITH ACCESSIBILITY SYMBOL
FACE OF WALKWAY/ EDGE OF PAVEMENT

NATIONAL ACCESSIBILITY SYMBOL PAINTED ON PAVEMENT WHITE FIGURE ON BLUE BACKGROUND

4" PAINTED STRIPING 1'-6" O.C. AT 45° IN FRONT OF RAMP (YELLOW REFLECTIVE)

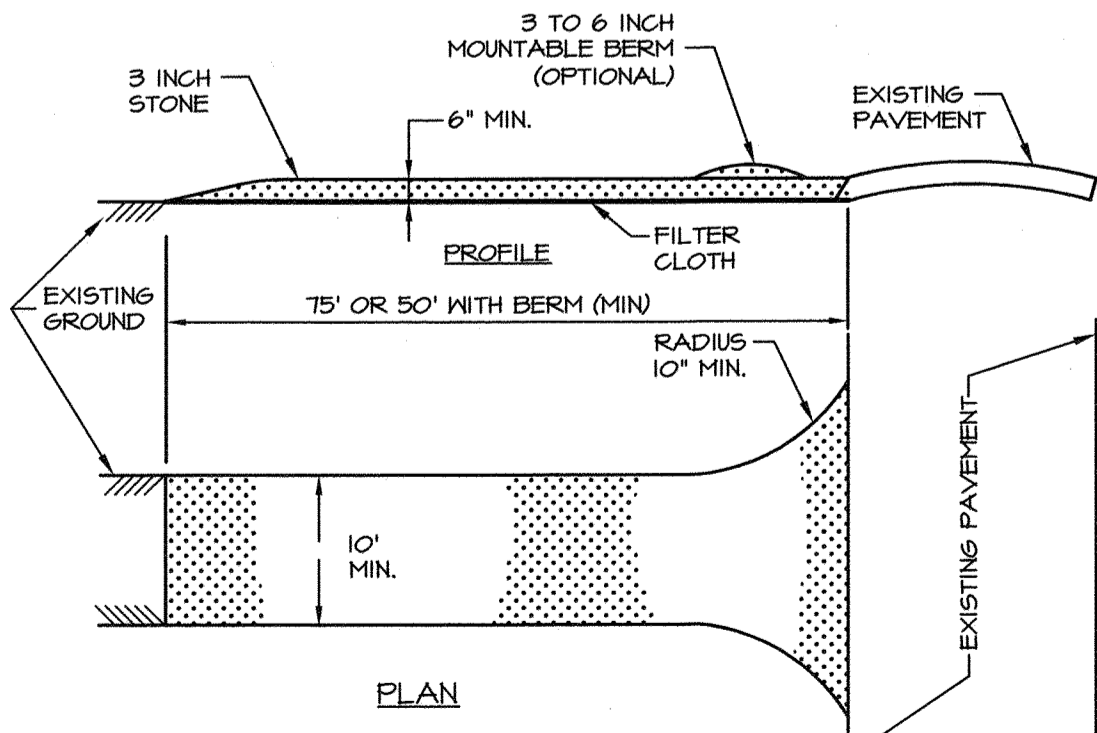
PAVEMENT SLOPE NOT MORE THAN 2% IN ANY DIRECTION

PARKING STALL FOR THE PHYSICALLY CHALLENGED
N.T.S.

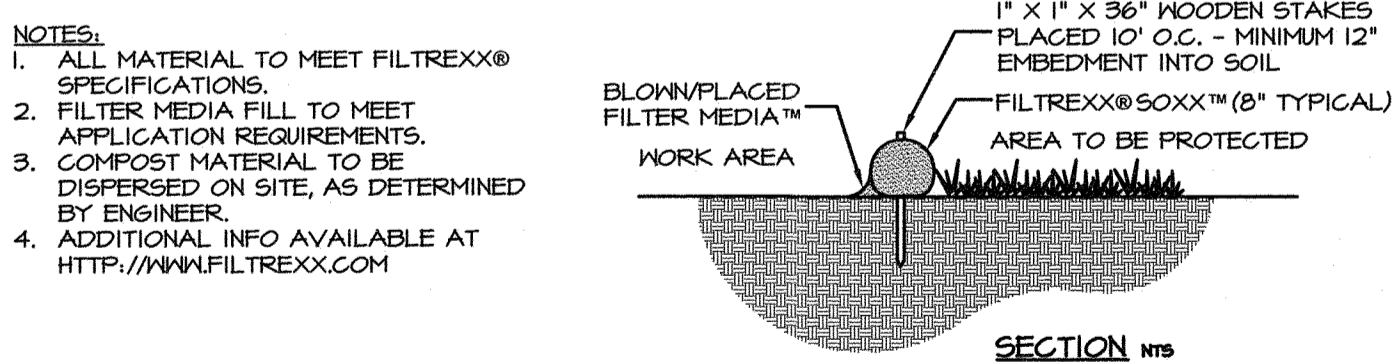


NOTE: SEE SHEET C2 FOR CLEANOUT LOCATIONS

UNDERDRAIN CLEANOUT DETAIL
NOT TO SCALE



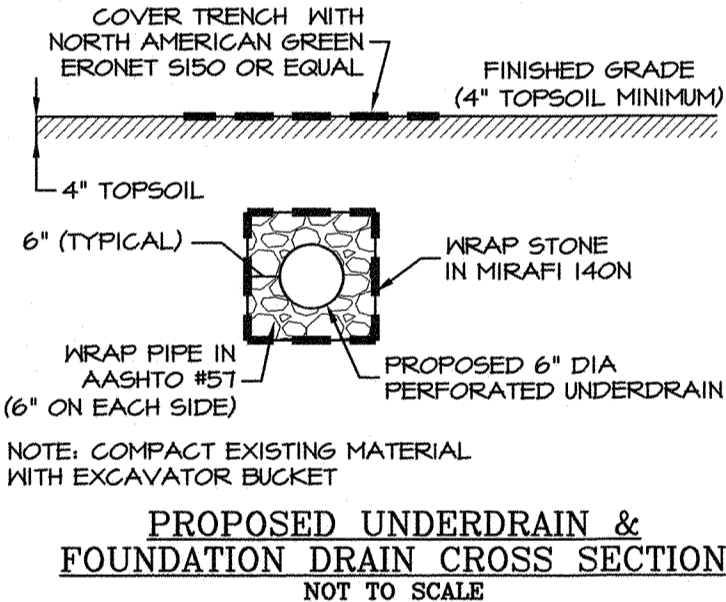
STABILIZED CONSTRUCTION ENTRANCE
N.T.S.



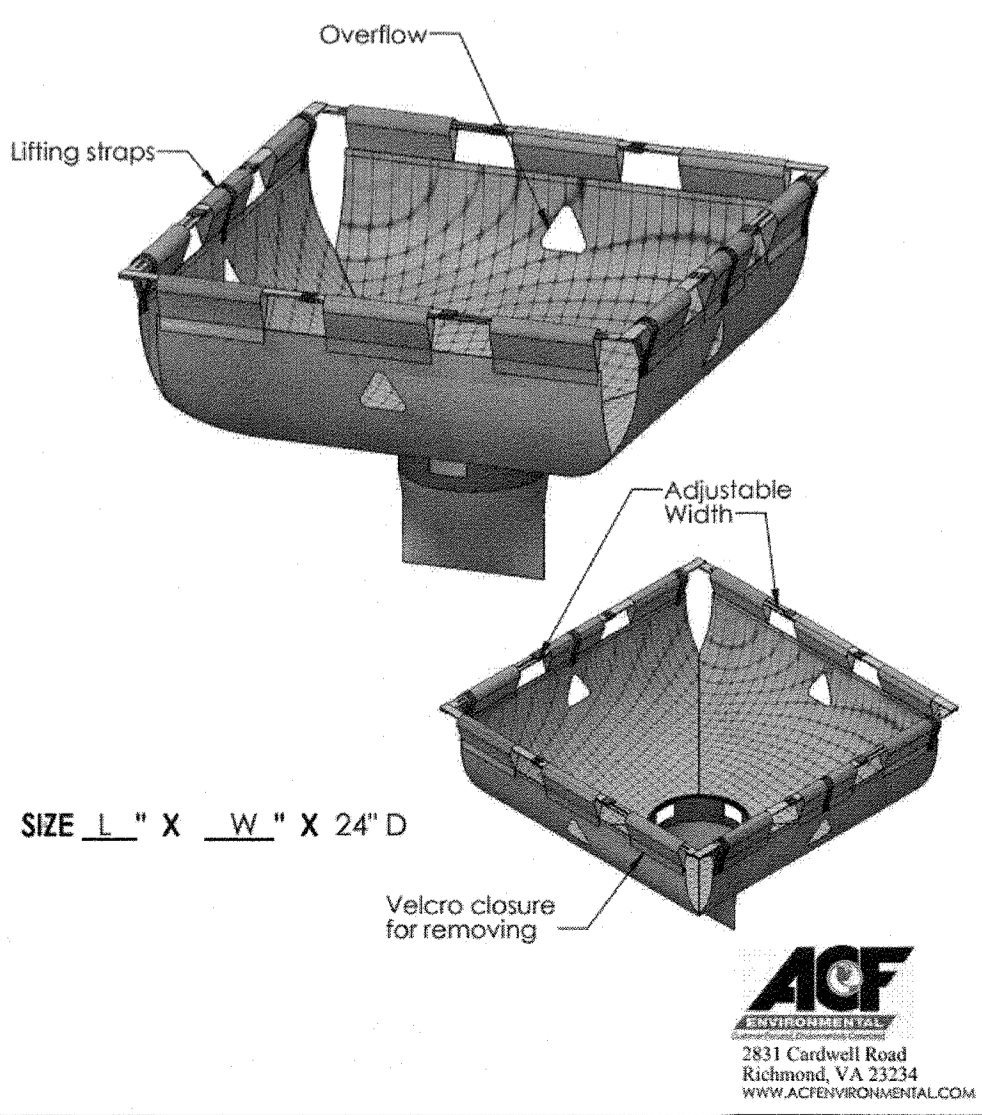
1" x 1" x 36" WOODEN STAKES PLACED 10' O.C.

FILTREXX® SOXX™ (8" TYPICAL)

FILTREXX® SEDIMENT CONTROL
N.T.S.



PROPOSED UNDERDRAIN & FOUNDATION DRAIN CROSS SECTION
NOT TO SCALE



SEAL

STATE OF NEW HAMPSHIRE
BRUCE D. SCAMMAN
No. 11236
LICENSED PROFESSIONAL ENGINEER

1 JAN 16, 2019 FOR APPROVAL

ISS. DATE: DESCRIPTION OF ISSUE: CHK.

DRAWN: JJM DESIGN: JJM

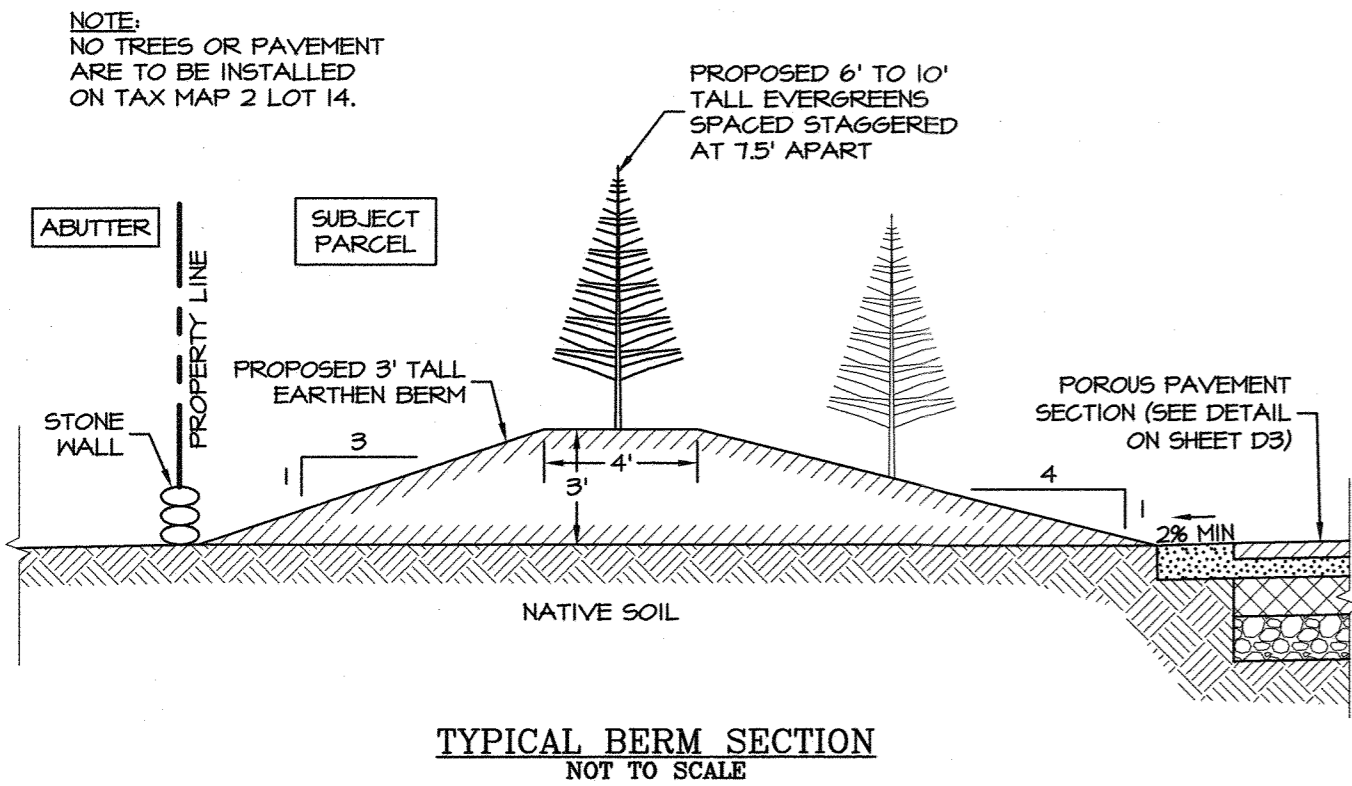
CHECKED: BDS CHECKED: BDS

EMANUEL ENGINEERING
civil & structural consultants, land planners
118 PORTSMOUTH AVENUE, A202
STRATHAM, NH 03885
P: 603-772-4400 F: 603-772-4487
WWW.EMANUELENGINEERING.COM

CLIENT:
SOPHIE ROBINSON
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

TITLE:
DETAILS FOR
ROBINWOOD CENTER
61 STRATHAM HEIGHTS ROAD
STRATHAM, NH 03885

PROJECT: 18-150 SCALE: AS SHOWN SHEET: D3



WALL MOUNT FIXTURE

MANUFACTURER: BETA LED
MODEL: SEC EDG 4M WM 08 C UL XX (COLOR)
LAMP WATTS / TYPE: LED
INITIAL LUMENS: 6677
MOUNTING: EXT WALL MOUNT AT 12 FEET
COORDINATE COLOR WITH STEEL MANUFACTURER OR ARCHITECT
COORDINATE WITH AVAILABLE VOLTAGE

LIGHT POLE

MANUFACTURER: HUBBELL SPAULDING LIGHT
MODEL: CLI-A-30L-1 U-5K-4 TYPEIV-DB-BL
LAMP WATTS / TYPE: LED
BL-BL LEVEL CONTROL
VOLTS: 240
LUMENS: 8088
LLF: 1.00
1/WATTS: 10
T/WATTS: 10
MOUNTING: 1" SQUARE POLE ON 3' BASE (20' TOTAL HEIGHT)
IDA APPROVED DARK SKY FRIENDLY FIXTURE
MOTION SENSOR
FULL CUT OFF

SEAL			
1 FEB 29 2019		FOR APPROVAL	
ISS. DATE:		DESCRIPTION OF ISSUE:	
DRAWN: JJM		DESIGN: JJM	
CHECKED: BDS		CHECKED: BDS	
CLIENT: SOPHIE ROBINSON ROBINWOOD CENTER 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885			
TITLE: DETAILS FOR ROBINWOOD CENTER 61 STRATHAM HEIGHTS ROAD STRATHAM, NH 03885			
PROJECT: 18-150		SCALE: AS SHOWN	
		SHEET: D4	



SOUTH ELEVATION | FRONT VIEW

SCALE: 1/4" = 1'-0"

NOTE: ALL EXISTING DIMENSIONS ARE TO ROUGH FRAMING / EXISTING POSTS WHICH ARE NOT CONSISTENT IN FIELD.
THEREFORE, FIELD VERIFY ALL DIMENSIONS.

preliminary set for bid set only - not for construction use.

REVISION TABLE		NUMBER	DATE	REVISED BY	DESCRIPTION

SCALED FOR:
SIZE D: 24 X 36

THE TULIP TREE, LLC
ROBINWOOD CENTER

© AMY DUTTON HOME
DRAWINGS USED
EXPRESSLY FOR DESIGN
ONLY FOR NOTED CLIENT. ALL
STRUCTURAL ENGINEERING
PROVIDED BY OTHER.

AMY DUTTON HOME
1 Walker Street, Kittery, ME 03904
email: amy@amyduttonhome.com
C: 207-337-2020

DATE:

1/16/19

SCALE:

1/4" = 1'-0"

SHEET:

A12

Dear Stratham Planning Department and Planning Board,

As an abutter at 9 Orchard Hill Road, I am writing to you to address the request for amendment to the site plan at 61 Stratham Heights Road property. Firstly, I would like to say that we would welcome any improvement to the plan that improves shielding, both of unsightly view, light and noise pollution. However, I do not see a concrete proposal here.

I would like to point out the original site plan was sorted out between abutters and the Saltonstalls over three years ago and was a result of attending multiple meetings of the planning board as well as multiple mediations between the abutters and the Saltonstalls. The trees required by the site plan have been out of compliance for the past two years. Now that they are finally being forced to address it, you are asking us to come to another meeting to discuss changing the plan with no clear proposal and no timeline.

The former residents abutting at 55 Stratham Heights Road chose to move specifically because of their dissatisfaction with the parking lot and associated zoning changes. The berm with more mature trees protecting 55 Stratham Heights Road are still in place, shielding that abutting property. That property also has two additional buildings shielding their home.

We wanted the berm to continue along the north side of the applicant's parking lot as well but only having the trees as a shield was a compromise. Perhaps if the berm was installed, all the trees shielding our home would have lived. Nonetheless, the trees should have grown several feet by now and actually be to the point where we wouldn't have to see an 80 car parking lot from our bedroom window.

Please see my key points below. I will reference the Site Plan Review Application page numbers if possible:

1. On pg#2. Suggesting why the trees died or that they can't be replaced, is unfounded personal conjecture. At the very least an arborist should have been consulted. Maybe the addition of a berm, or simply moving the trees a few feet away from the edge of the parking lot may have been the solution.
2. On pg#3. The specifics of the original site plan was created by the applicants, not forced on them by either the abutters or the planning board. The only direction from the planning board was that a berm and trees be added to shield the abutters.
3. On Pg#4. There was no "high quality property management plan" mentioned until now. In fact, when we very politely asked when they were going to replant the trees (last summer), they told us they didn't have to and that they planted some nice sunflowers there instead. They also suggested that if we were to pay to replant the trees, they would consider it. The only reason they are addressing this now is because the town is forcing them to.
4. Also on Pg#4. How does this "very special space" benefit our town far into the future and what does that have to do with shielding a parking lot? This parking lot only benefits the applicants.

5. On Pg#6. Corn is not an adequate replacement for maturing pine trees, which was the long-term goal. Corn will never grow tall enough to shield our second-floor bedroom windows from the parking lot, however, trees will. Corn provides no acoustic barrier and dies in the fall. Usage of the parking lot is not seasonally restricted.
6. Also on Pg#6. It is untrue to categorize this as “an ongoing conversation between my family, our abutters, and the Town of Stratham”. We are the only abutter that is no longer shielded and any attempt with a conversation has been rebuffed as mentioned above.
7. In general, the corn or the sunflowers and the “40’ of existing trees” that supposedly shield our abutting property are nearly all deciduous and are scant protection in September and October when the majority of their events take place. The only non-deciduous trees shielding us are evergreens that we personally added to our side of the property at significant expense. Also, the 40’ of existing trees and brush can simply be removed at the whim of the applicants. A fact that was made painfully plain to us when prior to their first big event last year, they cut down a significant portion of the shielding brush so that they could put up pup tents for their guests. There is nothing in the site plan keeping them from developing and changing the landscape along our property line at will. Is the planning board willing to restrict the applicants from removing any more trees or brush from along the property line?
8. The un-numbered pages providing photographs of sight lines from a distance in the showing the leaf-heavy foliage of spring/summer give a pointedly one-sided seasonal snapshot of the situation. Additionally, the sight lines (shown as red and blue arrows) are separate from the parking lot and have no bearing on the discussion.
9. The map showing the trees on the north side of the parking lot is from Google and is not from October of 2021, but from October of 2020, you can check it with Google Earth. Although the applicants may have grabbed the screen shot in October of 2021, the arborvitaes were removed prior to the 2021 season, making this current summer (2022) the second year the parking lot is not following the site plan.
10. The visual showing noise from the barn doors (regardless of being acoustically inaccurate) have no bearing on the parking lot site plan.

In summation, this has been an ongoing issue that needs action. The trees have been absent since 2020. No substantial effort has been made to address it, in fact just the opposite. The applicant’s request has no concrete plan and no timeline with which to be held. The amount of time and effort that the applicants are requiring of abutters and board members to attend additional board meetings, review additional proposals and mediate them is a frustrating waste of everyone’s time. If they want to implement a “high quality property management plan” then let them do so after they have made their parking lot compliant.

My final question is this. The site plan approved by the Planning Board in 2019 made the use of the parking lot contingent on the installation of the trees and a berm. Why are they being allowed to use this parking lot for two years out of compliance? From our point of view, making sure that there are trees to shield their neighbors from the nuisance of the parking lot is the absolute minimum that they could have done, and they haven’t.

Thank you to the Stratham Planning Department and Planning Board for your time and attention. We appreciate your ongoing patience, time and effort to attempt to resolve this issue.

Regards,
Joshua & Kimberly Cooper
9 Orchard Hill Road
Stratham, NH 03885



TOWN OF STRATHAM

Incorporated 1716

10 Bunker Hill Avenue Stratham, NH 03885

Town Clerk/Tax Collector 603-772-4741

Selectmen's Office/Administration/Assessing 603-772-7391

Code Enforcement/Building Inspections/Planning 603-772-7391

Fax (All Offices) 603-775-0517

Planning Board NOTICE OF DECISION

Petition of: Bruce Scamman, Emanuel Engineering, Inc.

Project Name: **Site Plan Application**, to facilitate installation of an off-street parking area, widening of a driveway, and associated infrastructure for the Robinwood Center, at 61 & 62 Stratham Heights Road, Stratham, NH; Stratham Tax Map 2 & 5 Lots 81 & 14; Exeter Tax Map 67 Lots 3 and 3-1.

Premises Affected: 61 & 62 Stratham Heights Road, Stratham, NH; Stratham Tax Map 2 & 5 Lots 81 & 14; Exeter Tax Map 67 Lots 3 and 3-1

So as to permit: The installation of an off-street parking area, widening of a driveway, and associated infrastructure for the Robinwood Center, at 61 & 62 Stratham Heights Road, Stratham, NH.

The Stratham Planning Board, at its meeting of March 06, 2019 and after a public hearing, completed its consideration of the application for the Site Plan Review application to installation of an off-street parking area, widening of a driveway, and associated infrastructure for the Robinwood Center, at 61 & 62 Stratham Heights Road, Stratham, NH, Tax Map 2 & 5 Lots 81 & 14; Exeter Tax Map 67 Lots 3 and 3-1.

The Board based its decision on plans, supporting oral and written information, and records provided by the Applicant, professional staff, consultants for both the Applicant and the Board, and abutters, as reflected in the minutes on file at the Stratham Town Hall. This information shall be incorporated into the decision by reference.

As a result of such consideration, at its meeting of March 06, 2019, the Planning Board found that the application for amendment was complete and in Substantial Compliance with the Stratham Zoning Ordinance and Site Plan Review Regulations. The Planning Board voted unanimously in favor to *Approve the Site Plan Review with Conditions* based on the information and stated conditions attached and incorporated hereto.

During the review process, the Applicant and its professional consultants submitted various revisions to the plans along with various supplemental memoranda and correspondence in response to requests by the Planning Board and the Planning Department that reviewed the project. All of these plans, reports and correspondence, and meeting minutes are contained in the Planning Department's files and are hereby incorporated by reference into the public record for this public hearing.

On March 06, 2019, Mr. Canada made a motion approve this application, as received on March 6, 2019, with the understanding that the operational notes as presented on Sheet C-2, specifically #1, #2, #3, #5, #6, and #8 be included in an advisory opinion memorandum to the Zoning Board of Adjustment, reviewed and approved by the planning board at the March 20, 2019 hearing, and operational notes #4 and #7 be relocated to the general operating notes in the upper right hand corner, and the February 6, 2019 staff review conditions of approval as the following:

Conditions Precedent:

1. Remove the cross walk and coordinate with DPW at a later date.
2. Applicant to coordinate with Town Assessing Department relating to current use stipulations and modifications on the property prior to recordation of the site plan.
3. Applicant to obtain NH DES septic approval prior to issuance of building permits.
4. Clerical changes of removing the snow storage from south side of Stratham Heights Road.

Mr. Canada amended the motion to include the Planning Board Chair is authorized to sign the advisory opinion memorandum instead it coming back to the board. Mr. House seconded the motion. Motion carried unanimously.



Planning Board Chair



Date



TOWN OF STRATHAM

Incorporated 1716

10 Bunker Hill Avenue · Stratham, NH 03885

Town Clerk/Tax Collector 603-772-4741

Select Board/Administration/Assessing 603-772-7391

Code Enforcement/Building Inspections/Planning 603-772-7391

Fax (All Offices) 603-775-0517

TO: Planning Board Members
FROM: Mark Connors, Town Planner
FOR: July 13, 2022
RE: **Potential Land Donation to Town – 18 Jana Lane**

Former Stratham residents Richard and Marilyn Young have offered to donate a small piece of land they own at 18 Jana Lane to the Town of Stratham (see their letter to the Select Board included in the Planning Board packets). The parcel is approximately 0.84 acres and includes 150-feet of frontage along Jana Lane. The property is interesting because it directly abuts the Salt River Association Conservation Land, which is protected by a conservation easement and allows for passive recreation use by the public (including hiking, cross country skiing, picnicking, and even boating access to the Squamscott River). Additionally, the Salt River land abuts several protected parcels along the Squamscott River owned by the NH Fish and Game Department, the Town of Stratham, and land protected by a Conservation Easement held by the NH Society for the Protection of NH Forests surrounding the Turnberry development (which although protected does not provide for public access).

It may be possible to construct a trailhead and small parking area at the 18 Jana Lane property to facilitate public access to these properties, but there are some challenges. There is a culvert that carries a brook across the property and the Town believes a good deal of the land may be wetland. It does not appear buildable for a traditional single-family house lot, but again there is a reasonable possibility that it could support a trailhead and some limited parking adjacent to the right-of-way. If construction of a trailhead would require a disturbance to or crossing of a wetland, the work would require a Dredge and Fill Permit from the NH Department of Environmental Services. Depending on the impact, some mitigation would likely be required.

The Youngs have indicated that there is some upland land on the eastern side of the lot. This cannot be known for sure until the land is surveyed and flagged by a certified wetland scientist.

The Select Board has requested the input of the Conservation Commission and Planning Board/Planning Dept. before responding to the Youngs. The Youngs have requested a response to their offer by August 1. The Youngs have provided the Town permission to walk the property and some members of the Conservation Commission have requested interest in participating in a site walk of the property. If members of the Planning Board are interested, staff can arrange for one in advance of the Commission's July 27 meeting.



TOWN OF STRATHAM

Incorporated 1716

10 Bunker Hill Avenue · Stratham, NH 03885

Town Clerk/Tax Collector 603-772-4741

Select Board/Administration/Assessing 603-772-7391

Code Enforcement/Building Inspections/Planning 603-772-7391

Fax (All Offices) 603-775-0517

TO: Planning Board Members
FROM: Mark Connors, Town Planner
FOR: July 13, 2022
RE: **Pubic Hearing – Adopt Route 33 HAC Application Fees**

The newly formed Route 33 Heritage Advisory Committee held their first meeting on June 16, 2022 and approved its own Rules of Procedure and Application form. The Advisory Committee discussed and recommended that the Planning Board adopt a fee structure as described below. At its June 22, 2022 meeting, the Planning Board discussed potentially increasing the application fees to better reflect staff and administrative costs. Potential fee amounts discussed at the previous meeting are shown below. If the Board is supportive of increasing the fees as previously discussed, staff has offered the following amounts shown in bold and in parentheses. Staff has kept the base fee for residential/agricultural applications the same at \$50.00 since many of these applications would not previously have required Planning Board applications and will not result in new housing units.

PROPOSED APPLICATION FEES:

Base Fee / Existing Residential/Agricultural Application: \$50.00

No commercial uses or additional housing units proposed.

New Residential Application: \$100.00* (**\$150.00**) + \$75.00* (**\$100.00**) per new housing unit

Where additional housing unit(s) is/are proposed.

Minor Commercial/Mixed Use Application: \$100.00* (**\$150.00**)

Adaptive re-use projects, architectural and/or exterior modifications or additions/new construction in which no more than 1,500 square-feet of additional interior space is proposed.

Major Commercial/Mixed Use Application: \$200.00* (**\$150.00 + \$100 per 1,000 of square-feet**)

In which more than 1,500 square-feet of additional interior space is proposed.

*-Site Plan and/or Conditional Use Permit application fees may also apply.

Since the Planning Board is the regulatory body for the Heritage District, the Planning Board must

approve the fee structure as part of a public hearing. At its June 22 meeting, the Board set the public hearing for July 13, 2022. The Planning Board has the option to see the fees however it sees fit. Staff will be prepared to discuss how other communities model their fees.

DRAFT MOTION:

I move to amend Section 4.2.7 of the Stratham Site Plan Regulations to set the fees for the Route 33 Heritage District as follows:

June 30, 2022

Stratham Select Board
C/O David Moore, Town Administrator
10 Bunker Hill Avenue
Stratham, NH 03820

RE: Potential land donation of 18 Jana Drive

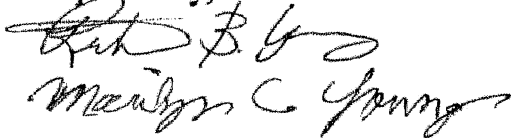
Dear Members of the Stratham Select Board,

We own property at 18 Jana Lane in Stratham (Tax Map 24, Lot 31). The lot is approximately 0.84 acres and includes approximately 150 feet of frontage along Jana Lane. We previously lived at the abutting lot at 20 Jana Lane but sold the property and relocated out of the area. If the Town is interested, we would like to donate the property to the Town. The lot abuts the Salt River Association Conservation Area and we believe it would be beneficial for conservation and/or recreational purposes. Additionally, there may be some benefit to the Fire Department to maintain equipment on the property to serve the Jana Lane/Joyce Lane/Jason Drive/Linda Lane neighborhoods.

Although we are willing to transfer the land to the Town at no cost, the transfer of the property would be contingent on the Town being responsible for costs associated with the property transfer, including associated taxes, legal or other professional fees, document preparation (including preparation of the deed) and other costs associated with the transfer and/or the Town's due diligence of this offer. However, if we choose to be represented by our own attorney, we recognize we would be responsible for those costs and we will remain responsible for the paying property taxes on the parcel up to the date of the property transfer.

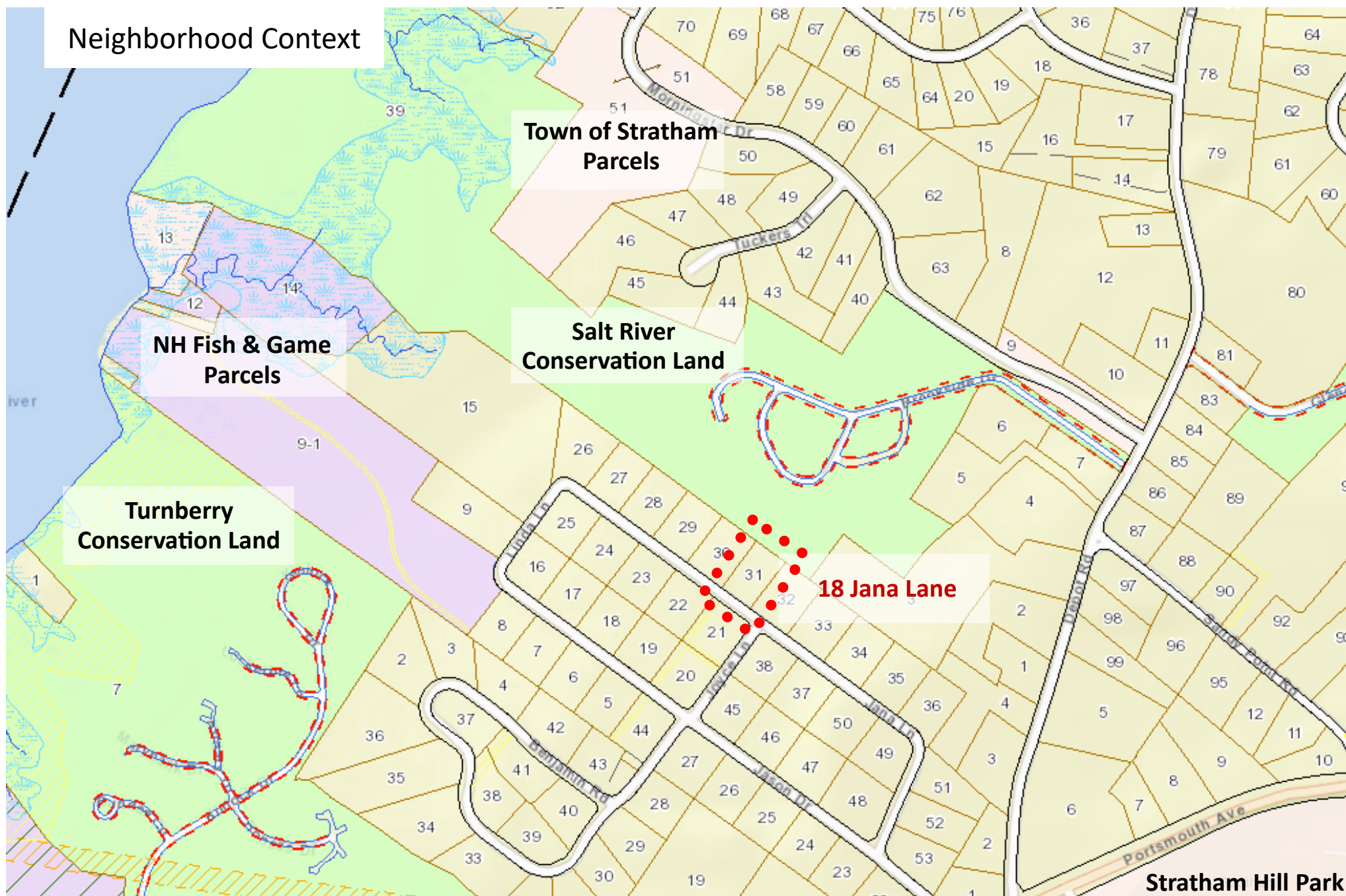
We recognize that the Board will likely want to conduct its due diligence and solicit the input of other Boards and Commissions, however we are anxious to move forward and would appreciate a response if the Town desires to accept the land donation no later than August 1, 2022. We understand that the transfer may not happen until a later date to allow for sufficient time to accommodate the need for public hearings and the preparation of the necessary transfer paperwork.

Respectfully,

Handwritten signatures of Richard and Marilyn Young. The first signature is for Richard Young, and the second is for Marilyn C. Young.

Richard and Marilyn Young

Neighborhood Context



Neighborhood Context



